

# Public Document Pack

## AGENDA FOR

## PLANNING CONTROL COMMITTEE



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**To: All Members of Planning Control Committee**

**Councillors :** G McGill (Chair), S Arif, C Boles, D Duncalfe, D Green, J Harris, M Hayes, D Quinn, S Thorpe, D Vernon and M Walsh

Dear Member/Colleague

### **Planning Control Committee**

You are invited to attend a meeting of the Planning Control Committee which will be held as follows:-

<b>Date:</b>	Tuesday, 26 July 2022
<b>Place:</b>	Council Chamber, Bury Town Hall
<b>Time:</b>	7.00 pm
<b>Briefing Facilities:</b>	If Opposition Members and Co-opted Members require briefing on any particular item on the Agenda, the appropriate Director/Senior Officer originating the related report should be contacted.
<b>Notes:</b>	<a href="https://councilstream.com/burycouncil">https://councilstream.com/burycouncil</a>

## **AGENDA**

### **1 APOLOGIES FOR ABSENCE**

### **2 DECLARATIONS OF INTEREST**

Members of the Planning Control Committee are asked to consider whether they have an interest in any of the matters on the Agenda and, if so, to formally declare that interest.

### **3 MINUTES OF THE MEETING HELD ON 28TH JUNE 2022** *(Pages 3 - 4)*

Minutes of the meeting held on Tuesday the 28<sup>th</sup> June 2022 are attached.

### **4 PLANNING APPLICATIONS** *(Pages 5 - 108)*

Reports attached.

### **5 DELEGATED DECISIONS** *(Pages 109 - 120)*

A report from the Head of Development Management on all delegated planning decisions since the last meeting of the Planning Control Committee is attached.

### **6 PLANNING APPEALS** *(Pages 121 - 128)*

A report from the Head of Development Management on all Planning appeal decisions since the last meeting of the Planning Control Committee is attached.

### **7 URGENT BUSINESS**

Any other business which by reason of special circumstances the Chair agrees may be considered as a matter of urgency.

**Minutes of: PLANNING CONTROL COMMITTEE**

**Date of Meeting:** 28 June 2022

**Present:** Councillor G McGill (in the Chair)  
Councillors S Arif, C Boles, D Duncalfe, D Green, J Harris,  
D Quinn, S Thorpe, D Vernon and M Walsh

**Public Attendance:** 5 members of the public were present at the meeting.

**Apologies for Absence:** Councillor M Hayes

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**PCC.1 APOLOGIES FOR ABSENCE**

Apologies for absence were submitted on behalf of Councillor M Hayes.

**PCC.2 DECLARATIONS OF INTEREST**

There were no declarations of interest made at the meeting.

**PCC.3 MINUTES OF THE MEETING HELD ON THE 31ST MAY 2022**

**Delegated decision:**

That the Minutes of the meeting held on the 31<sup>st</sup> May 2022 be approved as a correct record and signed by the Chair.

**PCC.4 PLANNING APPLICATIONS**

A report from the Head of Development Management was submitted in relation to applications for planning permission.

There was supplementary information to add in respect of application numbers 67586 and 68286.

The Committee heard representations from applicants and objectors in respect of applications submitted. This was limited to three minutes for the speaker.

**Delegated decisions:**

1. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and any supplementary information submitted with an additional condition for the installation of a EV charging point and subject to the conditions included:-

**11 Highfield Road, Prestwich, Manchester, M25 3AW**

Change of use of existing residential dwelling (Class C3) to residential institution (Class C2) with associated internal and external alterations

2. That the Committee **Approve with Conditions** the following application in accordance with the reasons put forward by the Development Manager in the report and any supplementary information submitted and subject to the conditions included:-

**Cosalea, Walshaw Park House, Walshaw Road, Bury, BL8 1PY**

Variation of conditions following approval of applications 63857 & 66758 to change existing opening hours; Condition 5 - Extension to existing opening hours requested; Monday - Thursday 07:30 - 11:00pm Friday 07:30 - 00:00 Saturday 08:00 - 00:00 Sunday 08:00 - 11:30pm

**PCC.5 DELEGATED DECISIONS**

A report from the Head of Development Management was submitted listing all recent planning application decisions made by Officers using delegated powers since the last meeting of the Planning Control Committee.

**Delegated decision:**

That the report and appendices be noted.

**PCC.6 PLANNING APPEALS**

A report from the Head of Development Management was submitted listing all recent planning and enforcement appeal decisions since the last meeting of the Planning Control Committee.

**Delegated decision:**

That the report and appendices be noted.

**PCC.7 URGENT BUSINESS**

No urgent business was reported.

**COUNCILLOR G MCGILL**  
**Chair**

**(Note: The meeting started at 7.00pm and ended at 7.34pm)**



<b>Title</b>	<b>Planning Applications</b>
<b>To:</b>	<b>Planning Control Committee</b>
<b>On:</b>	<b>26 July 2022</b>
<b>By:</b>	<b>Development Manager</b>
<b>Status:</b>	<b>For Publication</b>

### **Executive Summary**

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

### **This report has the following implications**

**Township Forum/ Ward:** Identified in each case.

**Policy:** Identified in each case.

**Resources:** Not generally applicable.

**Equality Act 2010:** All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:  
The elimination of discrimination, harassment and victimisation;  
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;  
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

**Human Rights:** All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

## **Development Manager**

### **Background Documents**

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

<b>1</b>	<b>Township Forum - Ward:</b> Ramsbottom and Tottington - Ramsbottom	<b>App No.</b> 66947
	<b>Location:</b> Land to east of M66 motorway, connecting Rochdale Road, Edenfield to Woodgate Hill WTW Compound, Bury	
	<b>Proposal:</b> Proposed works for and use of replacement section of aqueduct, including earthworks and ancillary infrastructure including: new valve house buildings and kiosk with permanent vehicular access provision; installation of tunnel shafts and open cut connection areas within a temporary construction compound, to include site access, storage areas, plant and machinery, drainage infrastructure and a residents' parking area. In addition, mine grouting works, to facilitate main tunnelling works.	
	<b>Recommendation:</b> Minded to Approve	<b>Site Visit:</b> N
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<b>2</b>	<b>Township Forum - Ward:</b> Radcliffe - East	<b>App No.</b> 67965
	<b>Location:</b> East Lancs Paper site, land between Cockclod Street/Cunliffe Street/Howard Street/Pollitt Street, Radcliffe Manchester, M26 9PG	
	<b>Proposal:</b> Residential development of 27 no. houses and apartments with car parking, landscaped areas and associated works	
	<b>Recommendation:</b> Approve with Conditions	<b>Site Visit:</b> Y
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**Ward:** Ramsbottom and Tottington -  
Ramsbottom

Item 1

**Applicant:** United Utilities

**Location:** Land to east of M66 motorway, connecting Rochdale Road, Edenfield to Woodgate Hill WTW Compound, Bury

**Proposal:** Proposed works for and use of replacement section of aqueduct, including earthworks and ancillary infrastructure including: new valve house buildings and kiosk with permanent vehicular access provision; installation of tunnel shafts and open cut connection areas within a temporary construction compound, to include site access, storage areas, plant and machinery, drainage infrastructure and a residents' parking area. In addition, mine grouting works, to facilitate main tunnelling works.

**Application Ref:** 66947/Full

**Target Date:** 30/07/2021

**Recommendation:** Minded to Approve

**This application is presented to the Committee as an application that has been submitted in accordance with Directive 2011/92/EU of the European Parliament and of the Council of 13 December 2011 on the assessment of the effects of certain public and private projects on the environment and the Town & Country Planning (Environmental Impact Assessment) Regulations 2017.**

**The Recommendation is Minded to Approve subject to referral to the Secretary of State for consideration and completion of s106 legal agreement in relation to Bio-diversity net Gain, ensuring public access to land / car parking and provisions relating to pedestrian access.**

## **Description**

### Overview

The application forms part of the overarching development of the Haweswater Aqueduct Resilience Programme (HARP), which involves the replacement of 6 underground tunnel sections of the 110 kilometre Haweswater Aqueduct that runs from Haweswater in the Lake District to Bury as part of the fresh water provision for Greater Manchester. The Haweswater Aqueduct carries water from the Haweswater Reservoir to a water treatment works near Kendal and then southwards to the borough of Bury. The HARP works comprise five new tunnel sections totalling a length of 53 kilometres across 7 different Local Planning Authorities. As such, a series of planning applications have been submitted for the scheme. The works in Bury form part of the Proposed Haslingden and Walmersley Section (PHWS) and applications have been submitted to neighbouring Hyndburn Borough Council, Rossendale Borough Council and this authority. An Environmental Statement to cover the PHWS has been submitted.

Each tunnel section would for constructional purposes have a driveshaft compound and reception shaft compound operating during the construction works. At the compounds connections between the existing aqueduct and the proposed tunnel sections would be constructed. Tunnelling would commence following the creation of the shafts from the driveshaft compound towards the reception compound. In broad terms the last driveshaft compound would be at Woodgate Hill, Bury. Spoil is removed from behind the tunnelling machine and as such the waste spoil would be extracted in Rossendale.

### Background to the application/need for the proposed works

The existing Haweswater Aqueduct became operational in the 1950s and transfers water from the Haweswater Reservoir in the Lake District National Park to a water treatment works near Kendal for treatment. From the water treatment works, the aqueduct conveys treated water to customers in Greater Manchester, Cumbria and Lancashire. Inspections carried out by United Utilities uncovered areas of concern in the single line sections of the aqueduct. It is anticipated that the condition of these sections will continue to deteriorate and would impact upon water supplies. As a statutory water services undertaker, United Utilities is required to supply drinking water that is safe and of a quality acceptable to customers.

United Utilities has been looking at different solutions, including:

- targeted repairs of the tunnel sections that are in the worst condition;
- replacement of the tunnel sections in the worst condition and provide targeted treatment for water quality
- construct new water treatment works at Bury and in the Ribble Valley and convert the aqueduct into a 'raw water' supply
- replacement of all Haweswater Aqueduct tunnel sections
- replacement of all Haweswater Aqueduct tunnel sections and provide additional water sources, including additional abstraction requirement).

The solutions were subject to Environmental and Social costings, Environmental Assessment, Habitat Regulations Assessment and Water Framework Directive Assessment to support the decision making. The preferred option to replace all the tunnel sections was approved by the Secretary of State in 2019.

The line of the tunnel within Bury extends from Rochdale Road near to the settlement of Turn. In this location the boundary with Rossendale Borough Council meanders around the proposed tunnel route resulting in three short length within the Metropolitan Borough of Bury. The pipeline then passes back into the Metropolitan Borough of Bury at Walmersley Golf Club and to the east of the M66 motorway, and then runs in parallel with the M66 until it reaches the existing Woodgate Hill WTW.

Woodgate Hill WTW compound is located off Castle Hill Road, to the east of the M66, approximately 2km from Bury Town Centre. The surrounding area comprises the urban areas of Bury and Walmersley to the west of the M66, and Fairfield, Jericho and Heap Bridge to the south of the compound.

### **Proposed Development**

The proposed development involves the works to construct a replacement section of aqueduct, including earthworks and ancillary infrastructure, including the following:

- New valve house buildings and kiosk with permanent vehicular access provision;
- Installation of tunnel shafts and open cut connection areas within a temporary construction compound, to include site access, storage areas, plant and machinery, drainage infrastructure and a residents' parking area.
- Mine grouting works, to facilitate main tunnelling works and grouting at some surface levels.

In Bury, the proposed development would provide 2.6 kilometres of replacement pipeline and would connect to the existing infrastructure. To enable the construction of the tunnel, the following temporary works would be required above ground:

- The proposed Woodgate Hill compound, serving tunnelling activities for the PWHS
- Woodgate Hill mine grouting area - to fill the existing voids in this area of former mine workings

- White Carr Lane (minor culvert works) – for construction access to White Carr Lane Mine Grouting Area (Rossendale)

The Woodgate Hill compound would be in place for 5 years with an estimated commencement date of 2025. The Woodgate Hill mine grouting area would be in place for 12 months with an earliest commencement date of 2023.

In Bury, the following permanent works are proposed at the Woodgate Hill compound:

- 2 access/valve house buildings with surrounding fencing and hardstanding.
- a kiosk and air valves to be provided at the downstream connection point
- stone access roads to the new access/valve house buildings and kiosk

#### Proposed tunnel section

The proposed development would replace 2 existing tunnel sections with an single tunnel, which would be 3.5 metres in diameter. The proposed tunnel would be constructed using a tunnel boring machine (TBM) below ground, which would work from Haslingden to the reception compound at Woodgate Hill in Bury. Once the new section of the aqueduct has been constructed, tested and commissioned, the sections of the old aqueduct would be decommissioned.

#### Temporary compound

The Woodgate Hill compound would house a reception shaft to receive the tunnel boring machine from Haslingden Road compound in Rossendale. The main construction area would require:

- the creation of a new temporary site access from Castle Hill Road
- Temporary car park for use by local residents affected by parking restrictions during construction works.
- Earthworks to create level areas and the creation of platforms for working machinery and haul routes where necessary
- Extensive areas of open cut works to connect into the existing infrastructure
- Temporary site cabins for offices, welfare, workshops and stores
- Car parking and commercial vehicle storage
- Plant, including a transformer, a generator, fuel tanks and large crane to lift the tunnel boring machine out of the main shaft.
- Traffic circulation routes
- Provision of a compound surface run off drainage and attenuation tank
- Material storage areas
- Site hedges fencing, including hoarding to 2.4 metres
- Shaft providing the connection to the Greater Manchester supply.
- Lighting

#### Mine grouting area

Mine grouting areas are required to minimise the potential for future mine collapse and to stabilise workings. Voids within the mine workings are infilled by introducing a liquid grout into them, which is delivered under pressure from ground prior to the area being tunnelled. This requires the drilling of a series of boreholes, through the soils and into bedrock to intersect the abandoned mine workings. Where possible, angled drilling is proposed to avoid disturbance of sensitive areas such as woodland, watercourses and Public Rights of Way (PROW).

The Woodgate Hill mine grouting area is 1250 metres long by 75 metres wide and would be accessed from the temporary access to the compound above and is anticipated to take 12 months to complete.

#### Permanent buildings/works

The proposed valve house and access buildings (north and south) would be constructed from brick with a profiled metal roof and would be located to the south and south east of the reservoir.

- A kiosk, measuring 7.5m x 7.5m and approximately 3.4m high and made of green painted steel (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00042)
- An access/valve house building (north), measuring 12m x 6m and approximately 4.7m high to the top of the pitched roof, with grey roof cladding and brick walls (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00043)
- An access/valve house building (south), measuring 11m x 8.5m and approximately 5m to the top of the pitched roof, with grey roof cladding and brick walls (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00044)

In terms of location there are two covered reservoirs on site. From reviewing the planning drawing the kiosk is south east of the western most covered reservoir. The two access/valve house buildings are to the east of the western most covered reservoir. (Refer to planning drawing 80061155-01-JAC-TR5-97-DR-C-00008). The existing operational stone access road will be extended to the new access/valve house buildings and kiosk.

#### Other Works

Tree and hedging removal would be required for site preparation and access works. There are no known PRowS affected by the Woodgate Hill WTW compound.

Woodgate Hill MGA – There are a number of footpaths and bridleways in the vicinity of the Woodgate Hill mine grouting area, PRow 42 would be temporarily closed, whilst Footpath 43 would remain open; though access gates would be in place where a section of this footpath is intersected by the construction access into the vertical drilling areas north and south of the footpath.

There would be limited material and waste arising as this would be a reception shaft and the shaft providing the connection to the Greater Manchester supply circa 25m deep. The bulk of the surplus material would be treated and removed from the Woodgate Hill WTW Compound. Material from the construction of the shafts, open-cut pipework and the connecting tunnel would be removed from site via the temporary access road to Castle Hill Road for disposal at a licensed facility or appropriate re-use.

#### Documentation

Given the scale and nature of the scheme, the application is accompanied with an Environmental Statement (ES). The ES covers many differing topics and the regulations seek to enable the ES to be objective in its consideration by identifying impacts, quantifying those impacts, considering mitigations and determination the likely effects of those mitigations in turn.

Topics covered within the ES include

- Landscape and Arboriculture
- Water Environment (including fluvial geomorphology, surface water quality and groundwater)
- Flood Risk
- Ecology
- Cultural Heritage
- Soils, Geology and Land Quality
- Materials and Waste
- Public Access and Recreation
- Communities and Health



- Major Accidents
- Transport Planning
- Noise and Vibration
- Air Quality and Climate Change
- Cumulative Effects and Interaction of Effects

Plans, reports and matrices have been submitted to demonstrate the scheme, its impacts and mitigations and summary documents where appropriate.

An extensive consultation process has been undertaken including notification of pre-application proposals by the developer and use of various interfaces to accommodate responses and comments as required by planning regulations.

In the run up to the submission scoping was undertaken to determine the main issues and characteristics of the development such that they can formulate the ES content. Numerous meetings have also taken place with statutory and non statutory consultees, again to inform the ES content.

### **Relevant Planning History**

50274 - 54km of part buried part over land pipeline connecting Woodgate Hill Reservoir in Bury to Prescott Reservoir, Knowsley (approx 9.51 km in Bury). Approved with conditions - 26 November 2008.

### **Publicity**

#### **Statutory Consultation and Publicity**

Under the EIA Regulations, the application was advertised in accordance with the following provisions:

Article 13 Publicity - Site notices 19/5/21 and press Notice on 20/5/21;

Regulation 19 - Secretary of State Notification 6/1/22;

Reg 25(5) - Receipt of Supplementary Environmental information - Press notice 3/3/22.

610 neighbouring properties were notified by means of a letter on 11 May 2021. As a result of this publicity three responses have been received.

1 letter of support has been received, which raised the following issues:

- Rochdale and Bury Bridleways Association would like to thank United Utilities and the HARP team for listening to our concerns and incorporating into their plans measures that have been suggested to ensure the safety of equestrians using the PROW and other routes in the areas in which they will be working whilst refurbishing the Haweswater Aqueduct.

2 letters have been received, which have raised the following issues:

- Thank UU for listening to resident concerns about parking and amending the application before submitting.
- Concerns about the volume of traffic that will be using Castle Hill Road (CHR) in terms of large vehicles for both deliveries/ removals and visitors/ workers accessing the site. Consideration to using the existing site entrance on 2nd Avenue/ 6th avenue for at least some of the traffic to reduce the impact on CHR.
- Castle Hill Road is poorly surfaced and traffic is noisy particularly when travelling at speed. The road surface needs to be improved and traffic calming measures/ speed limit in place to reduce potential noise pollution
- Consideration needs to be given to working with residents when work starts, particularly given the length of the programme. I would like reassurances of how we can get action

and the recourse available if there are problems such as early/ late working, traffic problems, noise etc.

The objectors and supporter have been notified of the Planning Control Committee meeting.

### **Statutory/Non-Statutory Consultations**

**Natural England** - No objections

**Traffic Section** - No objections subject to conditions and completion of agreements.

**Drainage Section** - No response.

**Environmental Health - Contaminated Land** - No objections, subject to the inclusion of conditions relating to contaminated land.

**Environmental Health - Air Quality** - No objections, subject to the inclusion of conditions relating to mitigation measures.

**Environmental Health - Pollution Control** - No objections subject to conditions

**Public Rights of Way Officer** - No objections subject to conditions and completion of agreements.

**Conservation Officer** - No objections.

**Waste Management** - No response.

**Environment Agency** - No objections subject to conditions

**Design for security** - No objections.

**United Utilities** - No response.

**The Coal Authority** - No objections, subject to the inclusion of conditions relating to coal mining.

**Transport for GM** - No objections

**GM Ecology Unit** - No objections, subject to the inclusion of conditions relating to bird nesting, bats, badgers, invasive species, ecological compensation, habitat and landscape management plans, ecological clerk of works and a legal agreement to secure the off-site habitat creation works proposed in the Environment Statement under biodiversity netgain.

**Natural England** - No objections.

**GM Archaeological Advisory Service** - No objections, subject to the inclusion of conditions relating to archaeology.

**Rochdale MBC** - No objections.

**Rossendale Borough Council** - No response.

**Minerals and Waste Planning Unit** - No response.

**Pre-start Conditions** - Applicant agreed with pre-start conditions.

### **Unitary Development Plan and Policies**

EN1/1 Visual Amenity

EN1/2 Townscape and Built Design

EN1/3 Landscaping Provision

EN1/11 Public Utility Infrastructure

EN2 Conservation and Listed Buildings

EN2/1 Character of Conservation Areas

EN2/2 Conservation Area Control

EN3/1 Impact of Development on Archaeological Sites

EN3/2 Development Affecting Archaeological Sites

EN3/3 Ancient Monuments

EN5 Flood Protection and Defence

EN5/1 New Development and Flood Risk

EN6 Conservation of the Natural Environment

EN6/1 Sites of Nature Conservation Interest SSSI's NNR's

EN6/2 Sites of Nature Conservation Interest LNR's

EN6/3 Features of Ecological Value

EN6/4 Wildlife Links and Corridors

EN6/5	Sites of Geological Interest
EN7	Pollution Control
EN7/1	Atmospheric Pollution
EN7/2	Noise Pollution
EN7/3	Water Pollution
EN7/4	Groundwater Protection
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/2	Woodland and Tree Planting
EN9	Landscape
OL1	Green Belt
OL1/2	New Buildings in the Green Belt
OL1/5	Mineral Extraction and Other Development in the Green Belt
OL4	Agriculture
OL4/1	Agricultural Land Quality
RT3/4	Recreational Routes
HT2/3	Improvements to Other Roads
HT1	A Balanced Transportation Strategy
HT2/4	Car Parking and New Development
HT2/6	Replacement Car Parking
HT4	New Development
HT6	Pedestrians and Cyclists
HT6/2	Pedestrian/Vehicular Conflict
HT6/1	Pedestrian and Cyclist Movement
GMMWP	Greater Manchester Minerals and Waste Plan
NPPF	National Planning Policy Framework

Draft National Policy Statement for Water Resource Infrastructure (Nov 2018) - These policies were published and issued for consultation by DEFRA with specific intentions of need for the development of water resource projects in England. This provides a primary basis for considerations under the planning regime in particular those that fall under National Infrastructure Projects. Those not falling under this regime, such as the current application is a matter for LPA's to consider rather than the Secretary of State. This therefore has capability of being a material planning consideration (NPPF para 5).

National Infrastructure Assessment 2018 and National Infrastructure Strategy(2020)  
The NPPF at para. 6 confirms that other statements of government policy may be material when preparing plans or deciding applications. The National Infrastructure Commission provides impartial advice on major long-term infrastructure priorities. The 2018 assessment aims to set out the next 30 year plan and recommendations, which this scheme is a part of. The NPPF reference therefore embraces other policy objectives thus these are capable of being material planning considerations.

### **Phasing of Works**

The construction Programme of Works is to be split into a number of phases

#### **Enabling Works**

- Vegetation clearance
- Site Access improvements
- Public Rights of Way temporary diversions
- Earthworks (to prepare the compounds)
- Establishing compound working areas including drainage and lighting
- Mine Grouting Activities
- Diversion of statutory undertakers' equipment where required

#### Construction Works

- Shaft construction
- Management of material/ surplus arisings
- Tunnel construction
- Construction of valve house and other necessary facilities
- Open-cut pipework construction (for connection to existing United Utilities infrastructure)

#### Commissioning Works

- Commissioning of new aqueduct and transition into operation
- Land reinstatement
- Decommissioning of existing Haweswater Aqueduct and reinforcement work.

Not all of these activities will commence at the same time but will by the use of conditional controls enable the scheme to be implemented in phases such that impacts are minimised and that there is a smooth transition between one stage and the next.

#### **Assessment:**

##### **Principle (Green Belt) and Visual impacts/impact upon openness**

The assessment shall be split into above ground works and below ground works.

#### Above ground

The application site is located in the Green Belt and as the proposals include above ground compounds and valve buildings, paragraphs 149 and 150 are relevant.

Paragraph 149 of the NPPF states that the construction of new buildings would be inappropriate in the Green Belt. Exceptions to this are:

- buildings for agriculture and forestry;
- the provision of appropriate facilities (in connection with the existing use of land or a change of use) for outdoor sport, outdoor recreation, cemeteries and burial grounds and allotments; as long as the facilities preserve the openness of the Green Belt and do not conflict with the purposes of including land within it;
- the extension or alteration of a building provided that it does not result in disproportionate additions over and above the size of the original building;
- the replacement of a building, provided the new building is in the same use and not materially larger than the one it replaces;
- limited infilling in villages;
- limited affordable housing for local community needs under policies set out in the development plan (including policies for rural exception sites); and
- limited infilling or the partial or complete redevelopment of previously developed land, whether redundant or in continuing use (excluding temporary buildings), which would:
  - not have a greater impact on the openness of the Green Belt than the existing development; or
  - not cause substantial harm to the openness of the Green Belt, where the development would re-use previously developed land and contribute to meeting an identified affordable housing need within the area of the local planning authority.

Paragraph 150 of the NPPF states that certain other forms of development are also not inappropriate in the Green Belt provided they preserve its openness and do not conflict with the purposes of including land within it. These are:

- mineral extraction;
- engineering operations;
- local transport infrastructure which can demonstrate a requirement or a Green Belt location;

- the re-use of buildings provided that the buildings are of permanent and substantial construction;
- material changes in the use of land (such as changes of use for outdoor sport or recreation, or for cemeteries and burial grounds); and
- development, including buildings, brought forward under a Community Right to Build Order or Neighbourhood Development Order.

Policy OL1/2 states that the construction of new buildings in the Green Belt is inappropriate development, unless it is for one of more of the following purposes:

- agriculture and forestry
- essential facilities for outdoor sport and outdoor recreation, for cemeteries, and for other uses of land which preserve the openness of the Green Belt and which do not conflict with the purposes of including land within it;
- limited extension, alteration or replacement of existing dwellings, provided that this would not result in a disproportionate additions over and above the size of the original dwelling, or, in the case of replacement existing dwellings, the new dwelling is not materially larger than the one it replaces;
- limited infilling in existing villages.

Policy OL1/5 states that within the Green Belt other development, not including buildings, will be inappropriate unless:

- it maintains openness and does not conflict with the purposes of including land in the Green Belt;
- in the case of mineral extraction, it does not conflict with the purposes of including land in the Green Belt, and high environmental standards will be maintained and the site well restored.

Proposals for other development not falling into one of the above categories is inappropriate development and is, by definition, harmful to the Green Belt. Any development proposal considered to involve inappropriate development will only be permitted in very special circumstances.

Proposed tunnel section, temporary compound and mine grouting works - The existing tunnel is located within the Green Belt and as such, the proposed tunnel section would inevitably be located within the Green Belt. The proposed construction of the underground tunnel sections, the mine grouting works and the temporary compounds associated with these works would be engineering operations, which is appropriate development in the Green Belt.

#### Compound Set up

The compound would be based around the Woodgate Hill reception shaft and would involve the creation of a temporary access off Castle Hill Road. The existing access off Second Avenue access/Sixth Ave would be used for up to 12 weeks whilst the new access off Castle Hill road would be constructed and for initial mine grouting works required prior to installation of the new access.

A temporary car park for local residents is included on Castle Hill Road. This reflects the potential for traffic restrictions where parking is displaced during the construction works. The car park would be accessed off a temporary access from Castle Hill Road.

Temporary cabins for offices, welfare and stores would be created as well as parking and vehicle storage in association with the development. Plant would be required, generators, fuel tanks, large crane and traffic circulation routes. Waste material would be carted from

site to appropriate reception sites and a surface water run off drainage scheme would be created to contain materials from the site.

Works ongoing at the Woodgate Hill WTW Compound would be for approximately 5 years. At Woodgate Hill MGA approximately 12 months and White Carr Lane MGA approximately 6 months.

Woodgate Hill WTW Compound totals approximately 172,000 m<sup>2</sup> within which there is a hoarded main works reception compound of approximately 7,000 m<sup>2</sup>. In addition within Woodgate Hill WTW Compound there are areas for anticipated shorter term occupation including areas for connection works, potential additional storage, temporary surface water outfall location and area for access to existing UU infrastructure.

The access road would be approximately 7m wide within a 30m wide red line planning boundary corridor. The access road is approximately 375m long with potential provision of approximately 450m<sup>2</sup> for a parking area for local residents adjacent to Castle Hill Road (only to be constructed if required).

The key consideration is that this element of the scheme is critical to the creation of the reception shaft that facilitates the boring machine. The works would be on site for not an insignificant period of time but are temporary, with the whole supporting infrastructure to be removed from the site and the land restored when complete.

Therefore, as a principle, given the nature of the proposed development and the way that this would be created and removed would not have a significant adverse impact upon the openness and character of the Green Belt and would be in accordance with Policies OL1/5 of the Bury Unitary Development Plan and the NPPF.

Permanent buildings/works - The proposed valve house buildings, access/valve house buildings, kiosk, air valves and access would not comply with the exceptions listed in paragraph 149 of the NPPF and as such, would be inappropriate development.

Paragraph 147 of the NPPF states that inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances.

Paragraph 148 of the NPPF states that when considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

- The works would comprise the tunnelling,
- A kiosk, measuring 7.5m x 7.5m and approximately 3.4m high and made of green painted steel (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00042)
- An access/valve house building (north), measuring 12m x 6m and approximately 4.7m high to the top of the pitched roof,
- An access/valve house building (south) measuring 11m x 8.5m and approximately 5m to the top of the pitched roof, with grey roof cladding and brick walls (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00044)
- Air valves to be provided at the downstream connection point (see drawing ref: 80061155-01-UU-TR5-97-DR-C-00047)
- Hard surfaced vehicular access and surfacing area for the new access/valve house buildings and kiosk.

The applicant sets out their case for very special circumstances in addressing inappropriate development and correctly relates these to the buildings and access ways above ground. Paras 143 and 145 of the NPPF confirms that these works amount to inappropriate development and therefore have the ability to harm and should only be allowed where harm is clearly outweighed by other considerations.

Given that the purpose of Green Belt is to safeguard against unrestricted sprawl and to prevent towns from merging the above surface works are required to be sited close to the infrastructure that they serve. The siting and nature of the proposed aqueduct is to serve the population at large with fresh water supply but for this to happen, very specific requirements of buildings, housing valves and supporting infrastructure must run alongside the aqueduct. This is therefore specific and very special by its very nature. The works can only be deemed to be related to the underground infrastructure and are relatively conservative in their size and design. The buildings would not give propensity to urbanisation and have little relevance to unrestricted sprawl or the merging of neighbouring towns. As such it is considered that the proposals comply with OL5/1 and the NPPF on these points.

#### Public Rights of Way

There are no known PRoW affected by the Woodgate Hill WTW compound, however there is a private right of access from Woodgate Hill Road to Ridings Farm that would be maintained.

Proposed tunnel section, temporary compound - The existing tunnel is located within the Green Belt and as such, the proposed tunnel section would inevitably be located within the Green Belt. The proposed construction of the underground tunnel sections, the mine grouting works and the temporary compounds associated with these works would be engineering operations, which is appropriate development in the Green Belt.

#### Mine grouting works

Mine grouting areas are proposed to minimise the potential for future mine collapse and to stabilise the workings at the tunnel horizons. Voids within the mine workings are therefore to be infilled by introducing a liquid grout into them, under pressure, from ground surface prior to the area being tunnelled. This requires the drilling of a series of bore holes, through the soils and into bedrock to intersect the target mineral horizons and abandoned mine workings. Wherever possible, angled drilling is proposed to avoid disturbance of sensitive areas such as woodland, watercourses and PRoWs.

The Woodgate Hill mine grouting area (see drawings BMBC-HW-APP-004-13\_02 and 03) is approximately 1250m long and 75m wide. Note that part of the access to the White Carr Lane mine grouting area (in Rossendale) is within Bury and shown on drawing BMBC-HW-APP-004-13\_01.

Woodgate Hill mine grouting area would be accessed via the new proposed temporary access from Castle Hill Road used to access the main Woodgate Hill WTW Compound. For up to 12 weeks the existing operations access road to Woodgate Hill WTW from Sixth Avenue via Second Avenue would be used whilst the access is being constructed. Some initial mine grouting works are also required in advance of the construction of the access.

Due to the nature of the works, the proposed mine grouting would be a shorter duration than the construction of the main tunnel. It is estimated that the grouting would take approximately 12 months to complete and would commence in circa 2023.

There are a number of footpaths and bridleways in the vicinity of the Woodgate Hill mine grouting area, PRoW 42 would be temporarily closed, whilst Footpath 43 would remain open; though access gates would be in place where a section of this footpath is intersected by the construction access into the vertical drilling areas north and south of the footpath

The mine grouting works would have temporary surface implications. However once completed, the works visually would have no impacts upon openness. Therefore, the proposed development would not have a significant adverse impact upon the openness and character of the Green Belt and would be in accordance with Policies OL1/5 of the Bury Unitary Development Plan and the NPPF.

### **Design and layout**

The proposed development would retain two access valve houses and a kiosk to operate the proposed aqueduct. The proposed access valve houses would be located to the east of the existing access road and would be constructed from brick with a grey pitched roof. There would be 3 louvres on two elevations and a single access, which would consist of a double set of doors in the gable elevation. The proposed development would be of a functional design and would not be a prominent feature in the streetscene.

The proposed valve kiosk would be located to the west of the existing access road and would be constructed from steel and painted green. Again, there would be louvres in two elevations and a single door. The proposed building would be of a practical design and would not be a prominent feature in the streetscene.

The proposed buildings would be over 130 metres from residential properties. As such, given the distance involved, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties in terms of loss of light or privacy.

Therefore, the proposed development would be in accordance with Policies EN1/2 and EN1/11 of the Bury Unitary Development Plan.

### **Landscape and Arboriculture**

NPPF - Section 12: Achieving well-designed places sets out the Government's approach for creating high quality buildings and places with an emphasis on good design. Paragraph 131 places emphasis on good design promoting 'high levels of sustainability' and 'weight should be given' when determining proposals that demonstrate this.

Section 15: Conserving and Enhancing the Natural Environment states the importance of contributing and enhancing the natural and local environment.

Paragraph 170 sets out that 'policies should contribute to and enhance the natural and local environment by 'protecting and enhancing valued landscapes...recognise the intrinsic character and beauty of the countryside'.

Paragraph 180 sets out that policies and decisions should ensure new development is appropriate for its siting, including that developments should 'limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes and nature conservation'.

Bury UDP Policy EN8: Woodland and Trees states that it will 'support the retention of trees, woods, copses and hedgerows and encourage natural regeneration and new and replacement tree planting'. Policy EN9: Landscape states that 'the Council will seek to protect, conserve and improve the landscape quality of the Borough'.

The ES includes a Landscape Visual Impact Assessment and it considers that the impacts arising from the scheme. It confirms that impacts would be significant where workings are to occur such as from the implementation of grouting works or the siting of compounds. This is to be expected. Such impacts would be prominent from short range and somewhat filtered by features within the compounds and on-going operations. In relation to residential views, the closest relationships such as from Castle Hill Road would be subject to more direct



relationships to development such as proximity to the temporary resident car park, access track whilst longer range views of the compounds would be mitigated by distance, topography and intervening vegetation. Overall, public users of PROWS or users of highways would experience the greatest of change due mainly to proximity. However the impacts are not considered to be prolonged or so significant to require mitigation, not least due to the temporary nature of impacts.

The clearance of vegetation would be required for site preparation and access widening works. These works would be completed outside of seasonal ecological constraints and re-instated as per the detail on the Environmental Masterplan (ref: HBC\_RBC\_BMBC-HW-FIG-020-001).

In total, the scheme would require 38 arboricultural features are at risk of removal which are of moderate to low quality; 20 individual trees, 16 tree groups, one hedgerow and one woodland. 77 arboricultural features are considered to be encroached upon but retained including a grade 'A' tree, a veteran tree and measures would be included to protect these features including exclusion zones, protective and precautionary measures. As detailed within the SEI there would be a further 1 arboricultural feature at risk of removal and 5 retained with protection measures associated with Woodgate Hill mine grouting area if considering the full loss in Bury.

The ES is clear in highlighting the losses and their locations. Assessments are also made that understands their significance. The impacts along the route are highly driven through routing of the aqueduct, locations of grouting, compound siting and access. The proposals would lead to inevitable loss somewhere along the route but the locations and actions have been minimised as much as reasonably possible. It is a requirement to understand the planning balance of the loss against the wider benefit of the proposals and whether the changes would be long lasting or not. The loss of woodland would be replaced and enhanced through the BNG proposals.

Penny Bennett was commissioned on behalf of Bury, Rossendale and Hyndburn Councils to independently assess the proposals in terms of landscape impacts. Comments were made in relation to the formulaic approach adopted within the ES and a lack of analysis of the different landscape character areas, details of mitigation planting e.g. trees, hedges etc.

The applicant submitted an update to the ES called Supplementary Environmental Information (SEI) documents. Its purpose was to directly respond to comments made by the respective LPAs and consultees on this and other issues. The SEI reinforced that the proposals are both seeking to restore affected land to 'the as was situation' and when read in conjunction with the Ecology elements of the proposals, deliver enhancement and uplift through Biodiversity Net Gain (BNG).

Overall, the nature of the impacts are considered to be not so harmful given the planning balance of the necessity of the scheme, the achievable ability to mitigate and the overall wider public benefit. The mitigations include replacement planting, land restoration and BNG. This would ensure at minimum of 10% net gain for lost biodiversity and a management period of 30 years on land within the control and ownership of the applicant.

On this basis, the impacts arising from the proposals within the ES are clear and mitigation is reasonable to ensure suitable off set. The proposals are therefore, with conditional controls through the use of planning conditions and any accompanying provision within a s106 agreement, compliant with EN8/2, EN1/1, EN6/4 of the Bury UDP and Chapter 15 of the NPPF.

## **Water impact issues**

The Flood Risk Assessment (ES Appendix 8.1) identifies that the Proposed Haslingden and Walmersley Section is classified as water transmission infrastructure and is therefore considered within the NPPF to be a water compatible development that is suitable in all areas of flood risk providing that it is safe, can operate in times of flood and does not increase flood risk elsewhere.

The Flood Risk Assessment concludes that the level of flood risk to the Proposed Haslingden and Walmersley Section is generally low from all sources of flooding. The application contains an overview of appropriate flood design principles, standards and best practice to be considered at later stages of the design process. With this appropriate embedded mitigation within the design of these assets and activities the Proposed Haslingden and Walmersley Section would remain safe from flooding and would not impact flood risk elsewhere.

Regarding sustainable drainage systems, the applicant's Land Drainage Statement sets out that the nature of the Proposed Haslingden and Walmersley Section is such that permanent above ground development is relatively minor with the main impacts of the development proposals being associated with the proposed temporary construction works. Therefore, ground infiltration methods such as swales and basins are not considered practical where proposed development land is to be returned to previous use on completion of development. Nevertheless, the proposals seek to discharge surface run off as high up the hierarchy of drainage options as reasonably practicable.

The proposals reference that drainage components, including culverts, the location(s) of which are shown on the accompanying mine grouting area drawings (for Bury - ref: BMBC-HW-APP-004-13\_01). It is intended for details of works affecting watercourses, site drainage proposals including surface water and groundwater management, culvert details and mitigation, to be confirmed in response to planning conditions which would require details to be submitted for acceptance prior to the relevant construction phase.

A Sustainable Drainage (SuDS) proforma has been completed and is included as within the application documentation.

The Environment Agency have been consulted on the proposals and were a part of the working group through the pre-application discussions. They confirm that they have no objections to the proposals but due to the lack of certain details, considered and recommended that conditions would resolve their concerns. These included surface water flows, quality control and management; discharge controls to any watercourses; decommissioning; aquatic ecology; valve access points and BNG.

In conclusion, with embedded mitigation implemented, the Proposed Haslingden and Walmersley Section is predicted to be safe from flooding and would not increase the risk of flooding elsewhere and with appropriate controls maintain an appropriate relationship of the works to the water environment. Therefore, it would comply with Paragraphs 155-163 of the NPPF and Policy EN5 of the Bury UDP.

## **Ecology**

Chapter 9A and 9B of the ES together with the Habitats Regulation Assessment and SSSI Report considers the potential terrestrial and aquatic ecology impacts associated with enabling works, construction, commissioning and operational phases along the route of the Proposed Haslingden and Walmersley Section. The section below describes the relevant effects for the Bury section which includes the Woodgate Hill WTW Compound and Mine Grouting Area.

The ES confirms that construction activities, for the enabling phase, in the absence of embedded and site specific mitigation have the potential to cause: physical loss of non-designated habitats (both permanent and temporary); damage, degradation or modification of retained habitats; fragmentation and isolation of retained habitats/network; habitat loss, exclusion, obstruction of movement and habitat fragmentation for mobile species; hydrological changes through ground disturbance; killing, injury or entrapment risk of fauna; disturbance of species through noise, dust, visual or vibration effects; and risk of introducing or spreading invasive species.

The ES confirms that construction activities in the absence of additional mitigation have the potential to cause: damage / degradation / modification of retained habitats as a consequence of changes to groundwater quality or pathways (flows/levels); damage / degradation / modification of retained habitats as a consequence reduced surface water quality from site run-off including sedimentation, or hydrological/geomorphological impacts including wash-out/erosion effects or encroachment within root protection areas of retained hedgerows and trees; killing, injury or entrapment risk of fauna; and disturbance of species through noise, dust, visual, lighting or vibration effects.

A series of broad based embedded mitigation measures are proposed to help reduce the significant impacts of construction on ecological features. These include targeted pre-construction surveys undertaken by an experienced ecologist to ensure previous data are still relevant, supervision of ecologically sensitive works by an Ecological Clerk of Works, temporary fencing to avoid incursion into sensitive adjacent habitats, implementation of the Environment Agency's Pollution Prevention Guidelines and adherence to industry-standard environmental safeguards as detailed in the CCoP and Arboricultural Method Statement (AMS). In addition, construction works would be undertaken wherever practicable outside of breeding and/or hibernation seasons (e.g. for birds, bats and amphibians) and not during the night time near sensitive habitats and species (e.g. for bats). Wherever practicable, all habitats would be restored to pre-construction conditions with elements of enhancement included where feasible.

Activities which may be required during the commissioning phase, such as access to valve house buildings and maintenance/inspections of the pipeline at well structure points, are unlikely to be of a scale, duration or nature that would give rise to significant ecological effects on ecology features. The commissioning discharges would be via existing United Utilities infrastructure. No impacts on aquatic ecology are expected from commissioning flows. As such the scheme would be compliant with NPPF Chapter 15 and EN6/3 and EN6/4 of the Bury UDP..

### **Highways issues**

Chapter 16 of the ES details an assessment of traffic and transport impacts on the local and strategic road networks from traffic associated with the proposed compounds and mine grouting areas required for the Proposed Haslingden and Walmersley Section during the construction period. The method of assessment has been agreed through discussions with the Local Highways Authority and National Highways as the Strategic Highways Authority

The assessment of traffic and transport impacts takes into account a representative construction period which reflects concurrent construction for the Proposed Haslingden and Walmersley Section. It comprises materials brought into the site for the purposes of construction, as well as materials removed from the site as waste. Assumptions have been made on the origins of imported material and the destinations of exported material, tunnel ring deliveries and other material deliveries according to possible locations of suppliers and resources. Vehicle routings have been defined by primary routes of the local road network as well as the Strategic Road Network providing north-south access.

In total, 9 traffic surveys were collected by Tracsis within the Proposed Haslingden and Walmersley Section during October and November 2019 over a 12-hour period on the local highway network, with data gathered for the Strategic Road Network through WebTRIS and DfT counts. A spreadsheet model was produced to assess the impacts of construction traffic against background flows, relative to the thresholds prescribed within the *Institute of Environmental Management and Assessment (IEMA)* Guidance on assessment of traffic environmental impacts. This has identified locations which are sensitive to changes in traffic noise particularly if tolerance thresholds are exceeded during the construction period when set against the baseline position (either 10% HGVs increase above existing or 30% total traffic).

There are no specific issues identified by the ES or Transport Assessment which considers that impacts would generally be “negligible to slight” when reviewed against the key indicators of severance, pedestrian delay and amenity. Larger volumes of traffic associated with construction are generally present where background traffic is already high therefore the overall impact of the Proposed Haslingden and Walmersley Section during construction is identified as minimal.

A mitigation strategy is proposed to reduce potentially slight impacts over a short period of time in locations which are most sensitive to an increase in traffic. They aim to ensure that effects on local receptors are limited, noting that the works are progressive and of mainly short term duration at a single location. The mitigation strategy includes:

- A CTMP has been produced which the contractor would update to include the most suitable access routes to / from locations chosen by the contractor(s) for the import of materials and the export of waste, to be agreed with the local highways authorities and National Highways;
- An Interim Travel Plan would help manage vehicle trips to / from the compound areas, which would ensure that car parking demand does not exceed beyond the limits of the compound onto neighbouring streets
- a Highways Stakeholder Group has been identified to ensure that concurrent construction operations associated;
- The need of with other major sites do not create significant cumulative impacts during any periods where parts of the local highway network may be closed due to the Proposed Haslingden and Walmersley Section.

The Transport Assessment has demonstrated that whilst some locations are sensitive to traffic increases the amount of additional traffic expected at these locations would only result in a slight change in local amenity. Furthermore, any impacts would only occur for a temporary period during construction and would not significantly increase journey lengths.

Transport for Greater Manchester (TfGM) and the Local Highway Authority (LHA) have been consulted on the proposals. TfGM have no objections to the scheme.

The LHA has localised concerns in relation to ensuring that pedestrian access can be provided along part of Castle Hill Road, the provision of a pedestrian refuge, how any displaced car parking would be dealt with and damage to highway surfacing infrastructure through increased usage.

The scheme provides for a pedestrian refuge along the required length of Castle Hill Road by using the applicant's adjoining land. This would be delivered through a legal agreement. The displaced parking facility would be provided by a small car park close to the entrance of Castle Hill Road for use by local residents. The applicant is offering and is accepted that an Extraordinary agreement under s59 of the Highways Act 1980 would be undertaken and this

is drafted. This would provide for a secured bonded fund to draw upon should works be required through the implementation of the scheme to public highways.

The provisions set out as mitigation are considered to be acceptable and would where necessary be conditioned or contained within appropriate legal agreements.

The quantum and direction of traffic arising from the scheme's implementation are understood and have been discussed at length through the pre-application discussions and these have manifested themselves within the application. Overall, the methodology of the implementation is such that since the drive of the tunnelling would be in a southerly direction, the impact arising from traffic would be significantly reduced. The main impacts would be from the creation of the compounds, staffing accessing and the creation of the reception shaft at Woodgate Hill.

Initially access would be via Sixth Avenue but the longer term routing would be from Woodgate Hill following the creation of an access. The applicant considered use of 2nd Avenue and 6th Avenue for the proposed works. These would be used initially whilst the new Castle Hill Road access road is constructed but were not deemed to be suitable for longer term use throughout the project which has been supported by Bury Highways team. The main concerns raised relating to the use of 2nd Avenue and 6th Avenue were with regard to the denser nature of the residential street, narrower width and steeper gradient compared to Castle Hill Road. Also 2nd Avenue is directly opposite a school with a school crossing patrol which was an added concern with this option.

The detailed programme of the works is unknown at this stage until a Contractor has been appointed. At that stage residents would be fully informed of the exact durations and start dates. The proposed works would not be continuous over the full 7 years (as stated by an objector). The timing of some activities is constrained by the remainder of the project – for example, mine grouting needs to take place prior to tunnel construction and final connections cannot take place until the tunnel is complete. More clarity on actual timing would be available once a Contractor is appointed.

Grouting would also be implemented as described above, which is a necessity and an area of Bentley Lane would also be impacted upon. Routing here is more problematic and narrow.

These factors have been considered and the wider public benefit weighs significantly in favour of temporary impacts. Public Rights of Way and Bridleways have been considered throughout and appropriate staff and community groups have been involved, resulting in changes for the positive and localised measures to be introduced where these are required. There would be localised impact upon amenity, users and a result of increased traffic movements through the implementation of the scheme. However, these would be temporary for the constructional phases but would cease upon completion.

In consideration of the reasons behind the scheme and the wider public benefits, the temporary nature of the scheme's implementation impacts and the direct routing to the main working area, the LHA and LPA have no objections and consider that the scheme is compliant with UDP Policies HT2, HT2/1, HT2/2, HT2/3, HT2/4, HT2/6, HT4, HT6, HT6/1, HT6/2, NPPF Chapter 8.

### **Air quality**

The ES has assessed the impact of the proposed development upon air quality and climate change agenda in the context of estimated carbon emissions.

Two assessment areas have been looked at:

- Local air quality up to 10km from the pollutant sources – relating to pollutants with the potential to affect human health and ecosystems at a local level during the construction phase.
- Dust during the construction phase up to 350m from the construction areas. A worst-case approach has been adopted to define the required level of good practice dust mitigation measures, to avoid any significant effects (i.e. it was assumed that all activities undertaken at the construction sites were categorised as high risk).

Assessment of the operational phase is not required as there are no known operational phase sources of air pollution.

The air quality assessment included consideration of the following aspects:

- Dust emissions generated by earthworks and construction-related activities during the construction phase.
- Exhaust emissions of pollutants to air from road vehicles (e.g. cars, vans, buses and lorries) on the local road network during the construction phase.
- Exhaust emissions from electrical generation plant (i.e. diesel generators) during the construction phase.

The main pollutants of concern have been identified as being the combustion emissions typically arising from vehicle traffic and industry, and are primarily oxides of nitrogen (NO<sub>x</sub>), nitrogen dioxide (NO<sub>2</sub>) and particulate matter (PM<sub>10</sub> and PM<sub>2.5</sub>). The emissions expected from on-site generators which include ammonia (NH<sub>3</sub>) and sulphur dioxide (SO<sub>2</sub>) have also been included as potential pollutants.

The detailed dispersal modelling results indicate that emissions to air from the diesel generators and exhaust emissions of pollutants from road vehicles are unlikely to result in any significant air quality effects at sensitive human locations or the designated sites in the assessment.

Appropriate dust mitigation measures have been identified to prevent significant impacts occurring at off-site locations. These measures are considered to be normal good practice that would be adopted by the contractor as part of the Construction Code of Practice.

The Environmental Health Section has no objections, subject to the inclusion of a condition relating to air quality mitigation measures.

Therefore, the proposed development would not have a significant adverse impact upon air quality and would be in accordance with Policy EN7/1 of the Bury Unitary Development Plan.

### **Noise and vibration**

The proposed development would generate noise and vibration through tunnelling to construct the aqueduct and the associated mine grouting works. These have been assessed in the Environmental Statement and has considered the impacts upon residential, non-residential properties and community assets

Noise arising from the activities at the southern reception shaft Woodgate Hill WTW compound has been assessed at 23 locations around the compound. The noise levels predicted would not give rise to significant effects at the majority of the residential properties close to the proposed compound. However, noise levels would be exceeded at properties to the west of the southern edge of the proposed compound and would impact on those on Castle Hill Road and Chaffinch Drive. In addition, there would be an impact during short duration enabling works and reinstatement works when piling and trenching are carried out and during tunnelling works on Sundays at Hole Bottom Cemetery Memorial Garden.

Mitigation measures include:

- temporary noise barriers during the enabling and reinstatement work, piling and open cut works near Hole Bottom Cemetery Memorial Garden and the affected residential properties
- partial or full enclosure of the pumps when undertaking work in close proximity to sensitive properties or community assets
- pneumatic hand tools would be fitted with efficient silencers
- Mine grouting - all pumps and generators located in the compounds and grouting areas would be screened with partial or full enclosure of items, where reasonably practicable
- Mine grouting - a 3 metre high topsoil mound would be located within the working area along the western edge of the three mine grouting areas and would provide screening for properties and community assets.

The proposed mitigation measures would reduce the impact of the proposed development for the majority of properties. However, there would be a moderate noise impact at No. 46 Chaffinch Drive during the connections, open cut works and piling and moderate noise impact at Hole Bottom Cemetery Memorial Garden during the enabling/reinstatement works, open and cut works. In addition, 108 Castle Hill Road would experience some moderate noise impact during enabling and reinstatement works.

Further comments will be reported from Environmental Health - Pollution Control Section in the Supplementary Report.

The ES stated that there was the potential for significant effects to residents some 40 metres from the southern edge of the compound during vibratory compaction works and the installation of sheet piling during trenching works. The SEI had provided further information on groundborne vibrations and it concluded that significant impacts, both in terms of human response and structural damage, resulting from groundborne vibration associated with the tunnelling works are not predicted. However, it may be perceptible at some properties. As such, the proposed development would not have a significant adverse impact upon the amenity of the neighbouring properties with regard to groundborne vibration.

### **Cumulative effects**

Chapter 19 presents an assessment of the potential for likely significant cumulative effects of the Proposed Haslingden and Walmersley Section. Part 1 of Schedule 41 of the EIA Regulations requires consideration of the cumulative effects of a development. IEMA (2011) notes that impacts can act together in an additive and/or synergistic manner to result in cumulative effects, i.e. impacts may overlap or act in combination with each other,

In the scoping exercise cumulative effects were stated to be required to be considered as is set out by the EIA Regulations and NPPF.

The scheme has looked at both the impacts arising from the development itself and those together with other non-related schemes and how these may impact. Each chapter of the ES considers different topic impacts as reflected in this report. To understand other non-related impacts, the applicant has looked at extant and schemes being implemented in the locality to add to the consideration of cumulative effects.

Each individual impact has appropriately commented upon cumulative effects individually in relation to the scheme applied for and also how other permissions elsewhere would have a relationship, if at all and whether these would create unacceptable cumulative impacts. In relation to the Bury element, the routing is such that LPA areas to the north including Rossendale and Hyndburn have been considered and also to the east, Rochdale LPA area.

The ES considers the overall project cumulative effects to be:

- Biodiversity loss
- Surplus materials
- Construction vehicle movements
- Tree loss

The combined biodiversity loss associated with the Proposed Programme of Works represents an additional potentially significant effect. To address this, United Utilities has committed to protecting certain habitats on construction compounds as well as habitat improvements equating to approximately 10% Biodiversity Net Gain. Given the mitigation in place across the Proposed Programme of Works and the overall net gain, the residual effect of the combined biodiversity loss is not considered to be significant. This also reflects the responses made by the consultee responses.

A review of all 'long 'list' proposed developments along all proposed sections of the Proposed Programme of Works identified that all proposals were 'local' in scope i.e. their environmental and social influence was confined to local areas and local communities. There were no third-party developments or proposed land allocations that could foreseeably act cumulatively with the Proposed Programme of Works and give rise to additional cumulative effects, over and above the likely significant environmental effects already described in the ES.

A staged assessment of cumulative effects has been undertaken for the Proposed Haslingden and Walmersley Section. These stages of assessment have included the consideration of local proposed developments and land allocations, identified with the assistance of local planning authorities, against the likely significant effects of the Proposed Haslingden and Walmersley Section. Where cumulative effects with local proposed developments and land allocations have been identified, none has been identified as giving rise to the risk of greater effects than those already considered in the individual topic assessments.

Combined effects across the full Proposed Programme of Works (i.e. all developments being undertaken as part of the Haweswater Aqueduct Resilience Programme) have been assessed. The combined potential habitat loss was assessed to constitute an additional potentially significant effect. Mitigation has been identified in the form of habitat protection and a commitment to generate a 10% Biodiversity Net Gain across the Proposed Programme of Works which would reduce the residual effect to not significant. No developments have been identified that would act cumulatively with the Proposed Programme of Works at a regional level.

This cumulative assessment therefore concludes that following mitigation there are no residual likely significant cumulative effects associated with the Proposed Haslingden and Walmersley Section.

### **Residual Effects**

Chapter 21 of the ES covers this issue as required by the EIA Regulations. The ES has broken the impacts down area dependently and provided commentary on each thus listing:

- Chapter of discussion
- Compounds
- Phase
- Commentary on residual effects



With Bury relationships, the following is stated:

Landscape and Arboricultural:

Haslingden Road Compound, New Hall Hey Compound  
Townsend Fold WTW Compound and White Carr Lane Mine Grouting Area

*Enabling Works:*

A noticeable and moderately uncharacteristic change to a small proportion of Rossendale Moorland Fringe LCA (Table 6.11)

- A dominant and uncharacteristic change across a large proportion of the view (Table 6.15: Haslingden Road Compound, New Hall Hey Compound Townsend Fold WTW Compound and White Carr Lane Mine Grouting Area, and Table 6.16: White Carr Lane Mine Grouting Area)
- A noticeable and uncharacteristic change across a moderate part of the view (Table 6.15: Haslingden Road Compound, New Hall Hey Compound Townsend Fold WTW Compound and White Carr Lane Mine Grouting Area, and Table 6.16: White Carr Lane Mine Grouting Area)

*Construction*

A substantial and moderately uncharacteristic change to a moderate or small proportion of the Rawtenstall TCA, Haslingden TCA and Irwell Valley South LCA (Table 6.19)

- A dominant and uncharacteristic change across a large proportion of the view (Table 6.22)
- A noticeable and uncharacteristic change across a moderate part of the view (Table 6.22)

*Operation (in year 1 of operation)*

A noticeable and uncharacteristic change across a moderate part of the view (Table 6.40). This would reduce to non-significant once mitigation planting becomes established.

Woodgate Hill WTW Compound, and Woodgate Hill Mine Grouting Area

*Enabling Works*

A noticeable and moderately uncharacteristic change to a moderate proportion of the Gorsey Brow LCA (Table 6.12)

- A noticeable and moderately uncharacteristic change to a small proportion of Rossendale Moorland Fringe LCA (Table 6.12)
- A dominant and uncharacteristic change across a large proportion of the view (Table 6.17)
- A noticeable and uncharacteristic change across a moderate part of the view (Table 6.17)

Woodgate Hill WTW Compound,

*Construction*

- A substantial and moderately uncharacteristic change to a large proportion of the Gorsey Brow LCA (Table 6.20)
- A substantial and moderately uncharacteristic change to a small proportion of the Knowl and Rooley Moors, Fringes and Foothills (Table 6.20)
- A dominant and uncharacteristic change across a large proportion of the view (Table 6.23)
- A noticeable and uncharacteristic change across a moderate part of the view (Table 6.23)

*Commissioning*

- A noticeable and moderately uncharacteristic change to a moderate proportion of the Gorsey Brow LCA (Table 6.26)

- A dominant and uncharacteristic change across a large proportion of the view (Table 6.29)
- A noticeable and uncharacteristic change across a moderate part of the view (Table 6.29)

#### Water Environment:

##### *Woodgate Hill WTW Compound Enabling Works*

##### *Groundwater Dependent Terrestrial Ecosystems (GWDTE):*

- Woodgate Hill WTW North (Table 7.39)
- Woodgate Hill WTW South (Table 7.39)

##### *Operation*

- Fluvial Geomorphology and Surface Water Quality:
- Discharge of groundwater ingress from the decommissioned Haweswater Aqueduct into the River Roch (Table 7.39)

#### Woodgate Hill WTW Compound

##### *Decommissioning (of existing aqueduct) Groundwater:*

- Changes to baseflow in Gypsy Brook during decommissioning of the existing aqueduct (Table 7.39)

#### Noise and Vibration

##### Woodgate Hill WTW Compound

##### *Enabling Works*

- Hole Bottom Cemetery Memorial Garden community asset where moderate adverse noise impact magnitudes (resulting in moderate significance effects) are predicted during enabling works/reinstatement works and open cut and piling works (Section 17.9)
- 108 Castle Hill Road (representative of ~9 residential properties on Castle Hill Road), where moderate adverse noise impact magnitudes (resulting in moderate significance effects) are predicted during enabling works/reinstatement works (Section 17.9)

##### *Construction*

46 Chaffinch Drive (representative of ~20 residential properties in the vicinity of Chaffinch Drive), where moderate adverse noise impact magnitudes (resulting in moderate significance effects) during connections, open cut works and piling. Moderate adverse vibration impacts are predicted during piling, resulting in moderate significance effects (Section 17.9)

#### Woodgate Hill WTW and Woodgate Hill Mine Grouting Area

##### *Enabling Works*

- Potential for combined significant effects during grouting works and enabling works at the Woodgate Hill WTW Compound, where noise levels in the vicinity of Chaffinch Drive are predicted to increase by 1 dB if concurrent mine grouting and enabling works are undertaken (Section 17.9)

#### White Carr Lane Mine Grouting Area

- White Carr Farm residential property, where moderate adverse noise impact magnitudes (resulting in moderate significance effects) are predicted during enabling works, drilling & grouting and reinstatement works (Section 17.9)

#### Woodgate Hill Mine Grouting Area

##### *Enabling Works*

- Hole Bottom Cemetery Memorial Garden community asset, with moderate adverse noise magnitude (resulting in moderate significance effects) predicted during enabling works, drilling & grouting and reinstatement works, and during enabling works at

Riddings Farm Cottage, Hercules Farm, Birchen Bower Farm and 46 Chaffinch Drive (and during reinstatement works at Riddings Farm Cottage). Major adverse noise magnitude (resulting in major significance effects) predicted at Riddings Farm Cottage, Hercules Farm, Birchen Bower Farm and 46 Chaffinch Drive during drilling & grouting works, and during site reinstatement works at Hercules Farm and Birchen Bower Farm (Section 17.9)

No Residual Effects upon:

Flood Risk  
Ecology – Terrestrial  
Ecology – Aquatic  
Cultural Heritage  
Solis, Geology and Land Quality  
Minerals and Waste  
Public Access and recreation  
Communities and Health  
Major Accidents  
Transport Planning  
Air quality  
Cumulative Effects / Arisings from other development

Each aspect of the development's topics has been thoroughly and objectively assessed in relation to residual effects. It is considered that there would not be any disagreement with the findings. Consultations carried out and the use of conditions and agreements would maintain the proposals to operate within reasonable and acceptable planning scope as required through this assessment and that the residual impacts would be acceptable as set out.

**Conclusion**

United Utilities manages the water supply network across the North West of England and has a statutory duty to supply drinking water that is safe and of a quality suitable for its consumers.

The existing Haweswater Aqueduct is approximately 110km long and consists of a number of underground water supply pipelines taking raw water from the Haweswater Reservoir in the Lake District National Park to a water treatment works (WTW) near Kendal, where it is treated (potable water) and then to United Utilities' customers in Cumbria, Lancashire and Greater Manchester.

Following detailed inspections of the tunnel sections of the existing aqueduct between 2013 and 2016, a number of the sections showed evidence of degradation that could lead to leakage and a risk to water quality. United Utilities is therefore proposing the Haweswater Aqueduct Resilience Programme (HARP) in order to replace the six existing underground tunnel sections, totalling a length of 53km, of the Haweswater Aqueduct, across seven local authorities, to provide a more resilient supply of clean drinking water,

As part of the Water Resource Management Plan (WRMP) process, which defines United Utilities' strategy to achieve a long-term, best value and sustainable plan for water supplies in the North West, the HARP Programme of Works has been through an extensive options identification and appraisal process to select the only feasible solution to address this risk to the North West's water supply. This document has been through an extensive consultation process with regulators and has been included within a WRMP approved by the Secretary of State and Ofwat.

The Programme of Works has been developed in conjunction with extensive stakeholder and community consultation and in particular regular liaison has taken place with the affected local planning authorities and other statutory consultees. Public consultation has

also been held involving public exhibitions, meetings with parish councils and other key stakeholders, and from April 2020, due to the Covid-19 pandemic, a digital platform was developed in order to continue with the public consultation and support the Proposed Programme of Works.

The Proposed Haslingden and Walmersley Section is the southern extent of the aqueduct and would involve replacing the water supply tunnels from Huncoat, east of Accrington, to the Woodgate Hill Water Treatment Works.

Within Bury, the development would comprise of approximately 2.6 km of replacement pipeline, including connections to existing United Utilities infrastructure.

- To enable the tunnel construction, above ground temporary works are proposed within Bury: The proposed Woodgate Hill WTW Compound, serving tunnelling activities for the Haslingden and Walmersley Section
- Woodgate Hill Mine Grouting Area – to fill existing voids in this area of former mine workings.

The Woodgate Hill WTW compound has an estimated duration of 5 years, with an estimated earliest commencement in 2025. It is estimated that the activity at the Woodgate Hill Mine Grouting Area would take approximately 12 months, with an earliest commencement date in 2023.

In addition to the new tunnel, the following permanent works are proposed: at the Woodgate Hill WTW compound area:

- 2 no. access/valve house buildings with surrounding fencing and hardstanding
- A kiosk
- Bound surfaced access roads to the new access/valve house buildings and kiosk

An EIA has been undertaken for the proposed Haslingden and Walmersley Section, which is reported in the accompanying ES. Its findings are summarised and assessed, in planning policy terms, within this Planning, Design and Access Statement. The nature of the Proposed Haslingden and Walmersley Section, where the development is predominantly under the ground, means the majority of the effects from the development on the surrounding environment and local amenity would occur during the construction phase of the tunnel rather than during the operation of the new proposed infrastructure. To account for this, the construction effects would be carefully controlled through a suite of documents, to be approved as part of the planning process, which set out measures to be implemented to ensure that any adverse effects on the environment, the landscape, public amenity and recreational opportunities etc. are minimised. These documents include

- An Outline Construction Code of Practice
- A Construction Traffic Management Plan
- A Mitigation Schedule
- An Environmental Masterplan.

Although it is recognised that the construction of a project of this scale would involve a degree of disruption to both people and the environment, the development proposals have objectively identified impacts and appropriate mitigation such that effects are appropriately determined. The proposals along with conditions are such that the effects can be reasonably controlled and that along with the mitigation and compensation proposals ensure that the

impacts are largely temporary and acceptable in planning policy terms and ultimately provide for a resilient, sustainable water supply to serve the needs of the North West.

Overall, it is considered that this essential upgrade to the water supply infrastructure is fully supported by national and local planning policies.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during pre-application discussions to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

**Recommendation:** Minded to Approve

### **Conditions/ Reasons**

1. The development must be begun not later than five years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered BMBC-HW-APP-004-01-01 0, BMBC-HW-APP-004-01-02 1, BMBC-HW-APP-004-02 1, BMBC-HW-APP-004-04\_01 0, BMBC-HW-APP-004-04\_02 0, BMBC-HW-APP-004-04\_03 0, BMBC-HW-APP-004-04\_04 0, BMBC-HW-APP-004-04\_05 1, 80061155-01-JAC-TR5-97-DR-C-00007 P03, 80061155-01-JAC-TR5-97-DR-C-00008 P01, BMBC-HW-APP-004-05\_01 0, BMBC-HW-APP-004-05\_02 1, BMBC-HW-APP-004-06 1, BMBC-HW-APP-004-07\_01 0, BMBC-HW-APP-004-07\_02 0, BMBC-HW-APP-004-800061155-01-UU-TR5-XX-DR-C-20023 P02, BMBC-HW-APP-004-09 0, 80061155-01-UU-TR5-DR-C-00042 P02, 80061155-01-UU-TR5-97-DR-C-00043 P02, 8006115-01-UU-TR5-97-DR-C-00044 P02, 80061155-01-UU-TR5-97-DR-C-00047 P01, BMBC-HW-APP-004-11\_01 1, BMBC-HW-APP-004-13\_01 1, BMBC-HW-APP-004-13\_02 1, BMBC-HW-APP-004-13\_03 0, BMBC-HW-APP-004-13\_04 0, B27070CQ-JAC-XX-DR-C-TR6\_GA-2206 P01.4, B27070CQ-JAC-XX-DR-C-TR6\_VS-2007 P01.4 and the development shall not be carried out except in accordance with the drawings hereby approved.  
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Prior to the commencement of development, a phasing plan showing the sequencing of the development for managing the construction, restoration and reinstatement works shall be submitted to and agreed in writing by the Local Planning Authority. Conditions (set out below) that are required to be submitted to the Local Planning Authority for approval shall be submitted to and agreed in writing by the Local Planning Authority in accordance with the sequence set out in the agreed phasing plan. The development shall thereafter be implemented solely in accordance with the approved scheme or as subsequently amended by written agreement with the Local Planning Authority.  
Reason. Details are required prior to the commencement of development in order

to secure satisfactory detailed delivery of the highway works within each phase of development and across the site as a whole in accordance with Policy HT1 - A balanced Transportation Strategy of the Bury Unitary Development Plan.

4. Notwithstanding the plans and details submitted, details for all the above ground structures associated with the respective compounds shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented during the course of the development.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

5. Notwithstanding the plans and details submitted, details for all the permanent above ground structures shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented during the course of the development.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

6. The respective compounds shall be removed from site to the written satisfaction of the Local Planning Authority within 6 months of it ceasing operation.

Reason: In the interests of the visual amenity pursuant to Policies H3/1 – Assessing Non-Conforming Uses and OL1/2 – New Buildings in the Green Belt of the Bury Unitary Development Plan.

7. No development within each phase approved under condition 3 shall commence unless and until:-

- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

8. Following the provisions of Condition 7 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

9. Development within each phase approved under condition 3 shall not be commenced until a Materials Management Plan for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any works to include works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation work. The materials management plan shall be developed following the site investigations and risk assessments and shall:
- a. Identify all locations from which material will be excavated;
  - b. Utilising the information contained within the contaminated land investigation, identify those areas of excavation which are contaminated
  - c. For areas of excavation which may be subject to contamination estimate the volume of material arising, the approximate volumes of material to be remediated on site and provisional volume to be disposed of off-site;
  - d. Illustrate where and how the remediation of contaminated material would take place;
  - e. Illustrate where and how remediated material would be re-used, including volumetric calculations to demonstrate that the material can be accommodated within the proposed area of use and any measures for containment for this material;
  - f. Detail the frequency of testing and testing specification for soils generated during the cut and fill operations, including how the materials are to be segregated and stored;
  - g. Identify screening criteria for assessment of whether the materials can be reused without treatment or mitigation;
  - h. For areas of excavation which are not subject to contamination provide the volume of material arising, and illustrate where and how non-contaminated material would be re-used including volumetric calculations to demonstrate that the material can be accommodated within the proposed area.

Once approved the materials management plan shall be implemented in its entirety.

Reason. To ensure the proposed development does not pose an unacceptable risk of pollution to controlled waters pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

10. Prior to the commencement of each phase approved under condition 3, a Site Waste Management Plan (SWMP) shall be submitted to and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation. The Site Waste Management Plan shall include details of:
- a. the anticipated nature and volumes of waste that will be generated by construction work;
  - b. the measures to minimise the generation of waste as a result of the proposed works;
  - c. measures to maximise the re-use on-site of such waste;
  - d. measures to be taken to ensure effective segregation at source of other waste arising during the carrying out of such works, including the provision of waste sorting, storage, recovery and recycling facilities as appropriate

The approved SWMP shall be implemented throughout the period of construction work on site.

Reason. To ensure the construction activities associated with the proposed development do not pose an unacceptable risk of pollution to controlled waters through the inappropriate management of waste on site pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

11. Prior to the commencement of construction work within each phase as approved under condition 3, a scheme detailing how surface water flows and quality will be controlled and managed during the construction phase of the development shall be submitted to and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any works to include works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation. The construction phase surface water management plan shall include the following and be implemented before construction starts:
  - a. An assessment of potential flows that would need to be managed during construction, including flows from the existing aqueduct, construction compounds and access roads and as a result of any groundwater dewatering or tunnelling activities;
  - b. Details of the measures which would be put in place to capture, manage, treat and discharge flows from the component parts of the site identified in part a.
  - c. A programme for the installation, maintenance and removal of the measures set out in part b. This should include provision for adapting the mitigation if it proves not to be effective.
  - d. An assessment of potential contaminants which may be present in surface water runoff, and measures to segregate this surface or ground water from clean runoff;
  - e. Assessment of potential options to retain, test and treat or remove potentially contaminated surface water runoff during the works;
  - f. Details of a monitoring scheme to be implemented to confirm that no contaminants are present in runoff from the site intended for discharge to controlled waters (before, during and post construction)
  - g. Details of how existing surface waters will be protected from any surface and ground waters generated

Once approved, the construction phase surface water management plan shall be implemented in its entirety and remain for the duration of the development. Should a need for amendments to the plan be required as a result of changing conditions, these must be submitted to and approved by the LPA.

Reason. To ensure the construction activities associated with the proposed development do not pose an unacceptable risk of pollution to controlled waters and associated species and habitats pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

12. No development within each phase as approved under condition 3, shall take place until a scheme for the provision and management of any compensatory habitat necessary to mitigate the impacts of the project has been submitted to, and agreed in writing by, the local planning authority and implemented as approved. Thereafter, the development shall be implemented in accordance with the



approved scheme.

Reason. To ensure the protection of wildlife and supporting habitats and secure opportunities for enhancing the site's nature conservation value and delivering biodiversity net gain pursuant to Policy EN6 - Conservation of the Natural Environment and the National Planning Policy Framework

13. No development within each phase approved under condition 3, shall take place until a scheme to ensure that;
- All private water supplies that may be impacted by the proposed development have been identified and any measures necessary to mitigate the impacts of the development on them have been agreed with the LPA;

Thereafter, the development shall be implemented in accordance with the approved scheme.

Reason. To ensure that the proposed development does not harm the water environment in line with paragraph 174 of the National Planning Policy Framework

14. Development within each phase approved under condition 17 above shall not be commenced until a Traffic Management Plan (TMP) for the relevant phase has been submitted and approved in writing by the Local Planning Authority in association with the Local Highways Authority. The TMP shall include:
- latest works programme, to allow coordination and understanding of cumulative impact of other programmed works in the area (linked to Condition 17 above)
  - The construction site traffic routes to be controlled by condition, the TMP should include a plan that clearly identifies each route
  - Detailed design of Castle Hill Road Temporary Access

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

15. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved by the Local Planning Authority and shall confirm/provide the following:
- Hours of operation and number of vehicle movements;
  - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
  - Parking on site of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials;
  - Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations

The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of

vehicles and storage of demolition/construction materials.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

16. The routing of construction site traffic to/from the site will at all times, unless otherwise agreed in writing by the Local Planning Authority in consultation with the Local Highways Authority, be restricted to the following routes set out below:

**Woodgate Hill WTW Compound**

M66 Junction 2 either from the north or south, Rochdale Road, Derby Way, B6222 Moorgate, B6222 Bell Lane, B6222 Rochdale Old Road, Castle Hill Road.

[For up to approximately 12 weeks, via Second Avenue and then Sixth Avenue whilst the access on Castle Hill Road is constructed]

**White Carr Lane Mine Grouting Area**

From/to north Edenfield roundabout, A56 Wood Lane, A56 Whalley Road, A56 Manchester Road, A56 Walmersley Road, Walmersley Old Road, Bentley Lane, White Carr Lane.

From/to south M66, A56 Walmersley Road, Walmersley Old Road, Bentley Lane, White Carr Lane.

No other routes are to be used by site traffic unless otherwise agreed in writing by the Local Planning Authority in consultation with the Local Highways Authority.

Reason. To avoid pedestrian and highway conflict in relation to the construction of the development pursuant to Policy HT6/2 - Pedestrian/Vehicular Conflict of the Bury Unitary Development Plan.

17. Development within each phase approved under Condition 17 above shall not be commenced until a Construction Travel Plan for the relevant phase has been submitted to and approved in writing by the Local Planning Authority in association with the Local Highways Authority. The approved details shall be implemented before the respective phase commences.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent streets pursuant to Policy HT4 - New Development of the Bury Unitary Development Plan.

18. Development within each phase approved under Condition 3 above shall not be commenced until details of the location, design and specification of wheel-cleaning facilities or other measures to prevent the tracking out of material or debris onto the public highway for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. The wheel cleaning facilities or other measures approved pursuant to this condition shall be installed and thereafter maintained in working order and be used by all Heavy Goods Vehicles leaving the site throughout the construction and reinstatement phases.

Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy EN1/2 - Townscape and Built Development and Policy HT4 - New Development of the Bury Unitary Development Plan.

19. Development within each phase approved under Condition 3 above shall not be commenced until details of the location, design and specification of wheel-cleaning facilities or other measures to prevent the tracking out of material or debris onto the public highway for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. The wheel cleaning facilities or other measures approved pursuant to this condition shall be installed and thereafter maintained in working order and be used by all Heavy Goods Vehicles leaving the site throughout the construction and reinstatement phases.  
Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy EN1/2 - Townscape and Built Development and Policy HT4 - New Development of the Bury Unitary Development Plan.
20. Development within each phase approved under Condition 3 above shall not be commenced until the final details of each compound layout for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented.  
Reason. Information not submitted at application stage. To ensure adequate measures are in place for each phase of the proposed development pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.
21. No development shall take place within each phase approved under condition 3 until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by the local planning authority. The WSI shall cover the following:
1. Informed by the updated North West Regional Research Framework, a phased programme and methodology of investigation and recording to include:
    - i. archaeological evaluation trenching (based on the trenching plan supplied to Jacobs by GMAAS on 10th March 2021)
    - ii. informed by the above, more detailed targeted excavation (subject of a new WSI).
  2. A programme for post investigation assessment to include:
    - iii. analysis of the site investigations records and finds
    - iv. production of a final report on the investigation results
  3. Deposition of the final report with the Greater Manchester Historic Environment Record
  4. Dissemination of the results commensurate with their significance.
  5. Provision for archive deposition of the report and records of the site investigation.
  6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.
- Reason. To record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible pursuant to Policies EN3/1 – Impact of Development on Archaeological Sites, EN3/2 – Development Affecting Archaeological Sites and EN3/3 – Ancient Monuments of the Bury Unitary Development Plan and Section 16 of the NPPF.
22. No development within each phase as approved under condition 3 shall commence unless or until the mitigation measures for dust and air quality referred to in the Environmental Statement have been implemented and are available for

use.

Reason. To ensure the proposed development would not have an adverse impact upon air quality pursuant to Policy EN7/1 - Atmospheric Pollution of the Bury Unitary Development Plan.

23. No tree felling or vegetation clearance required to facilitate the scheme should be undertaken during the optimum time for bird nesting (March to August inclusive) unless nesting birds have been shown to be absent by a suitably qualified person.

Reason. All nesting birds, their eggs and young are legally protected under the terms of the Wildlife and Countryside Act 1981 (as amended) pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15 .

24. Any trees scheduled for removal as part of detailed plans for the scheme should be further inspected for the possible presence of bat roosts before any felling work commences. Surveys must be carried out by suitably qualified persons and to appropriate standards. If bats are found, measures must be proposed for the avoidance of harm to bats and for compensatory provision of replacement roosting sites.

Reason. All UK bats and their resting places are legally protected under the terms of the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species regulations 2019 (as amended) pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15

25. A precautionary survey of the works area for Badgers must be undertaken prior to work commencing. If the presence of badgers is recorded, a method statement must be prepared giving details of measures to be taken to avoid any possible harm to badgers

Reason. Badgers are mobile in their habits, and are protected under the terms of the Protection of Badgers Act 1992. There is suitable habitat in the works areas to support badgers pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15.

26. A detailed management / method statement must be provided giving details of measures to be taken to prevent the spread of the invasive plant Himalayan balsam during the course of the scheme.

The approved management / statement plan shall include a timetable for implementation and be submitted prior to ground works, affected by such invasive species, takes place. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.

Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

27. Measures proposed for mitigating and compensating for the ecological harm which the scheme will cause as set out in the Environmental Statement (and in particular in the Construction Code of Practice (Appendix 3)) and Chapter 9 (Terrestrial and Aquatic Ecology)) are to be required to be implemented as approved.

Reason. in the interests of protecting wildlife and the natural environment pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15.

28. Comprehensive Habitat and Landscape Management plans must be required to

be prepared for new on-site landscapes and habitats. The Management plans and proposals should include details of the mechanism{s} by which the long-term implementation of the plan will be secured by the developer with the management body(ies) responsible for its delivery.

Reason. in the interests of landscape and habitat quality and sustainability pursuant to Bury UDP Policies EN6 and NPPF Chapter 15.

29. An Ecological Clerk of Works must be required to be appointed to oversee all aspects of the enabling, construction and reinstatement works.

Reason. in the interests of protecting wildlife and the natural environment pursuant to Bury UDP Policies EN6/3 and NPPF Chapter.

For further information on the application please contact **Helen Leach/Dave Marno** on **0161 253 5432**



66947

Aerial 1

66947 - Land to west of M66 motorway, connecting Edenfield to Bury

User Name



66947

Aerial 2



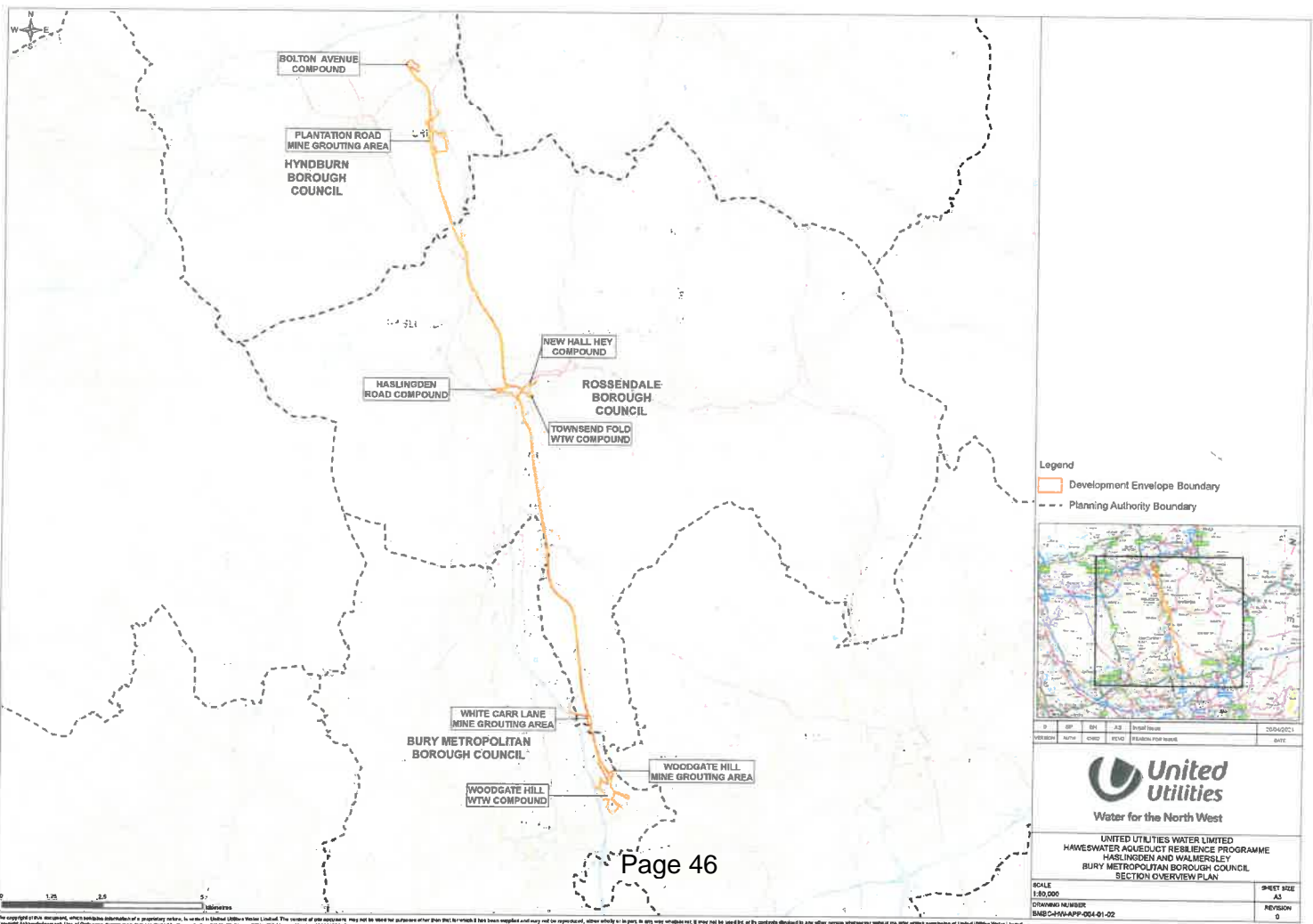
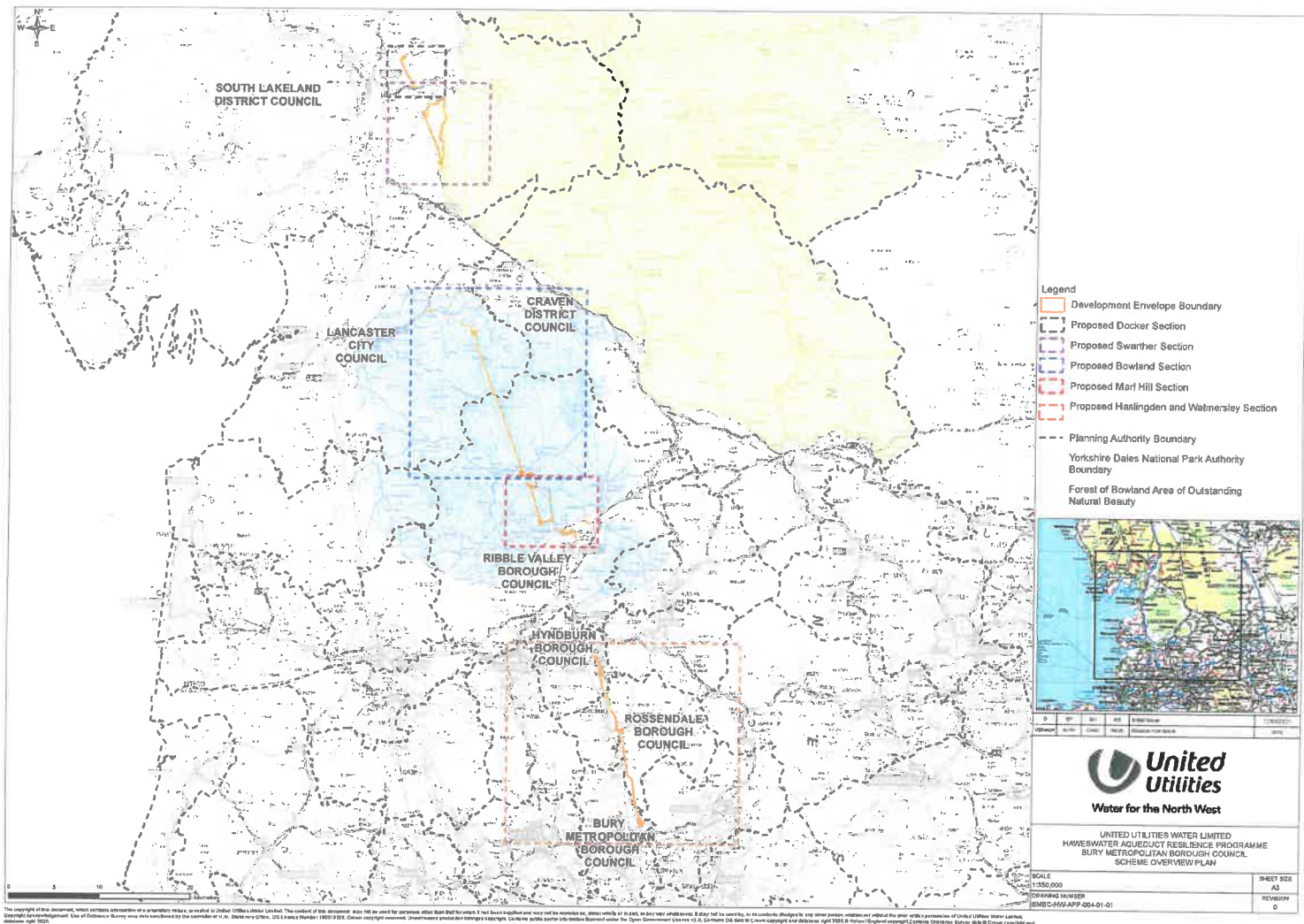


66947

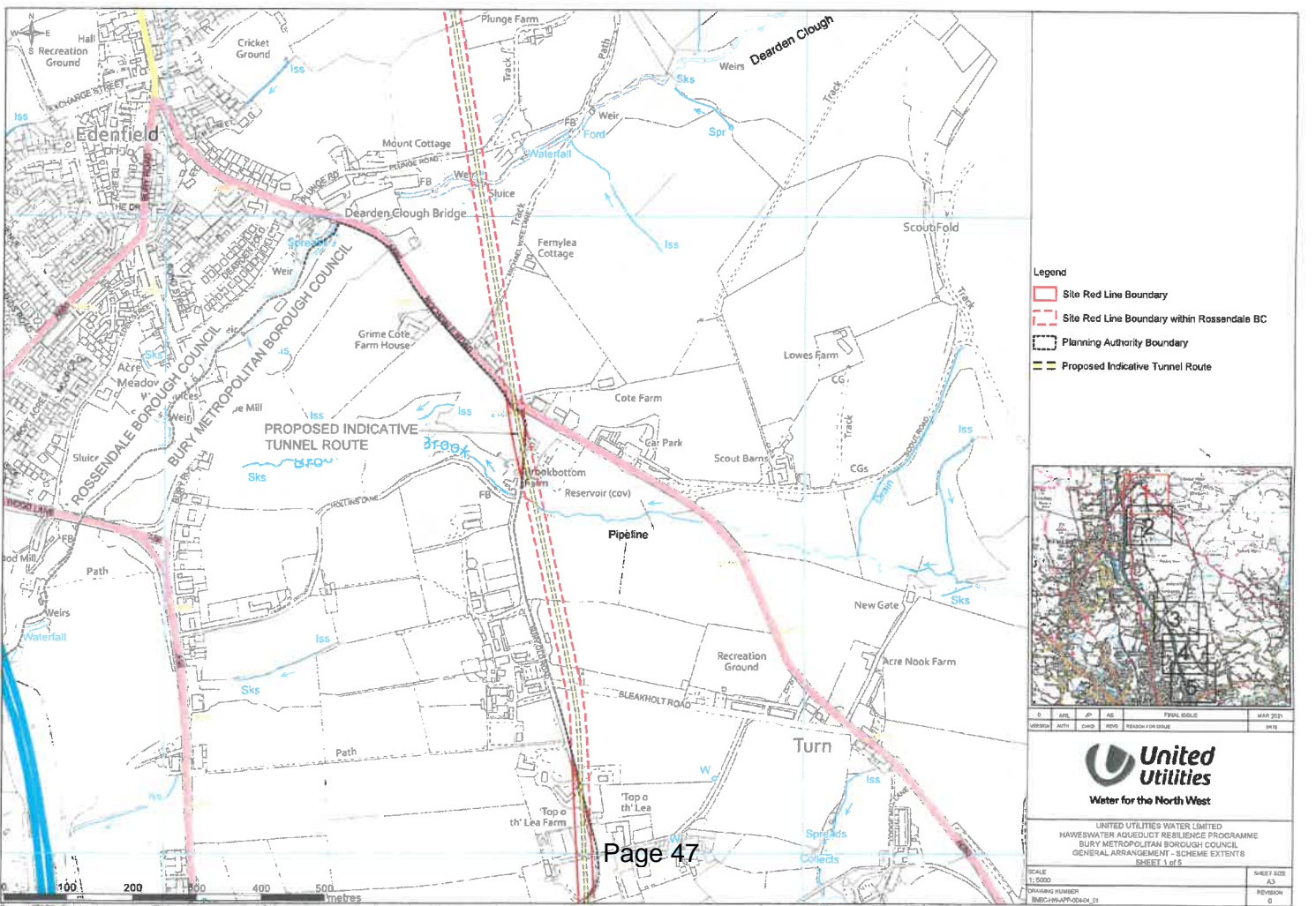
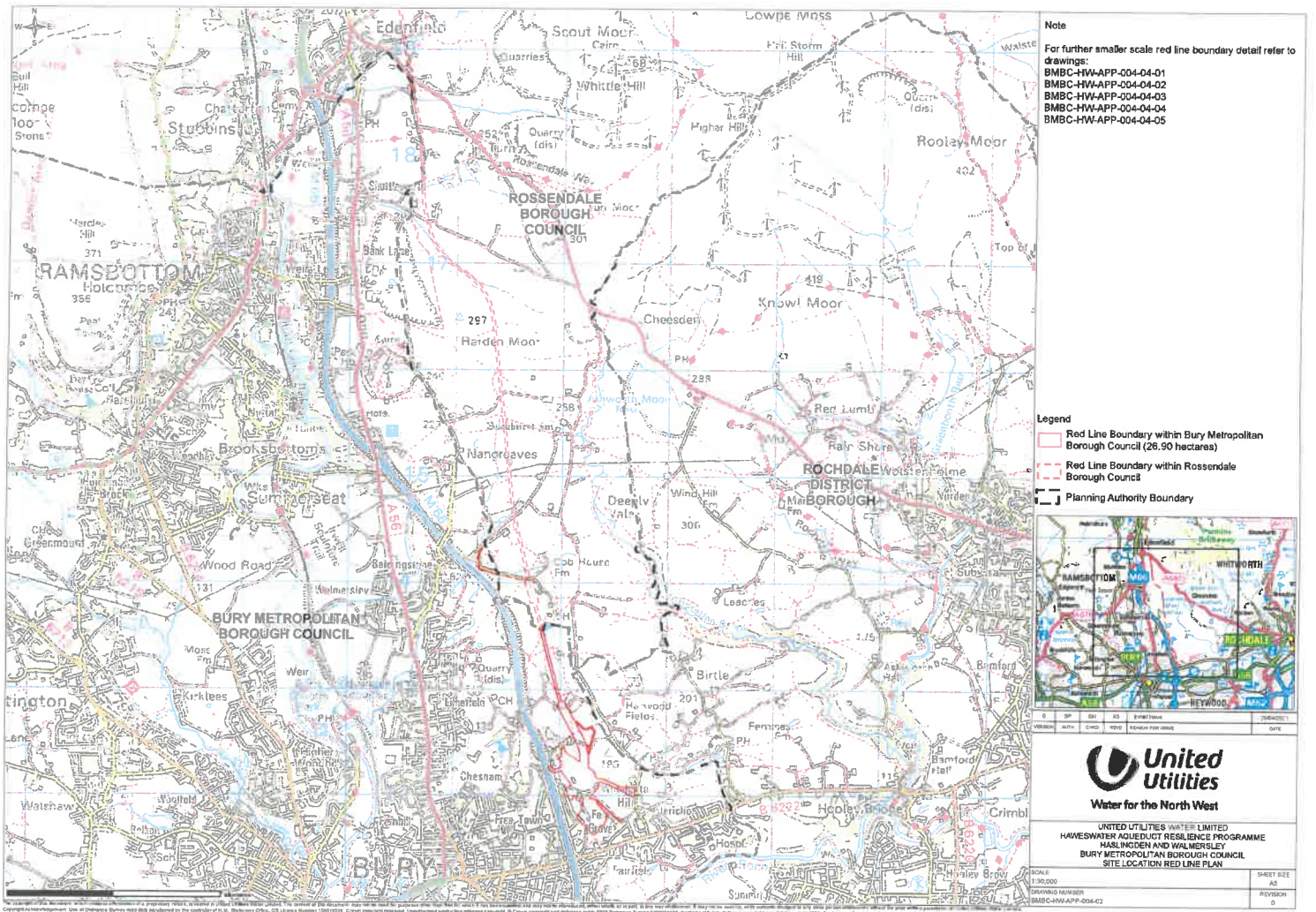
Aerial 3

66947 - Land to west of M66 motorway, connecting Edenfield to Bury

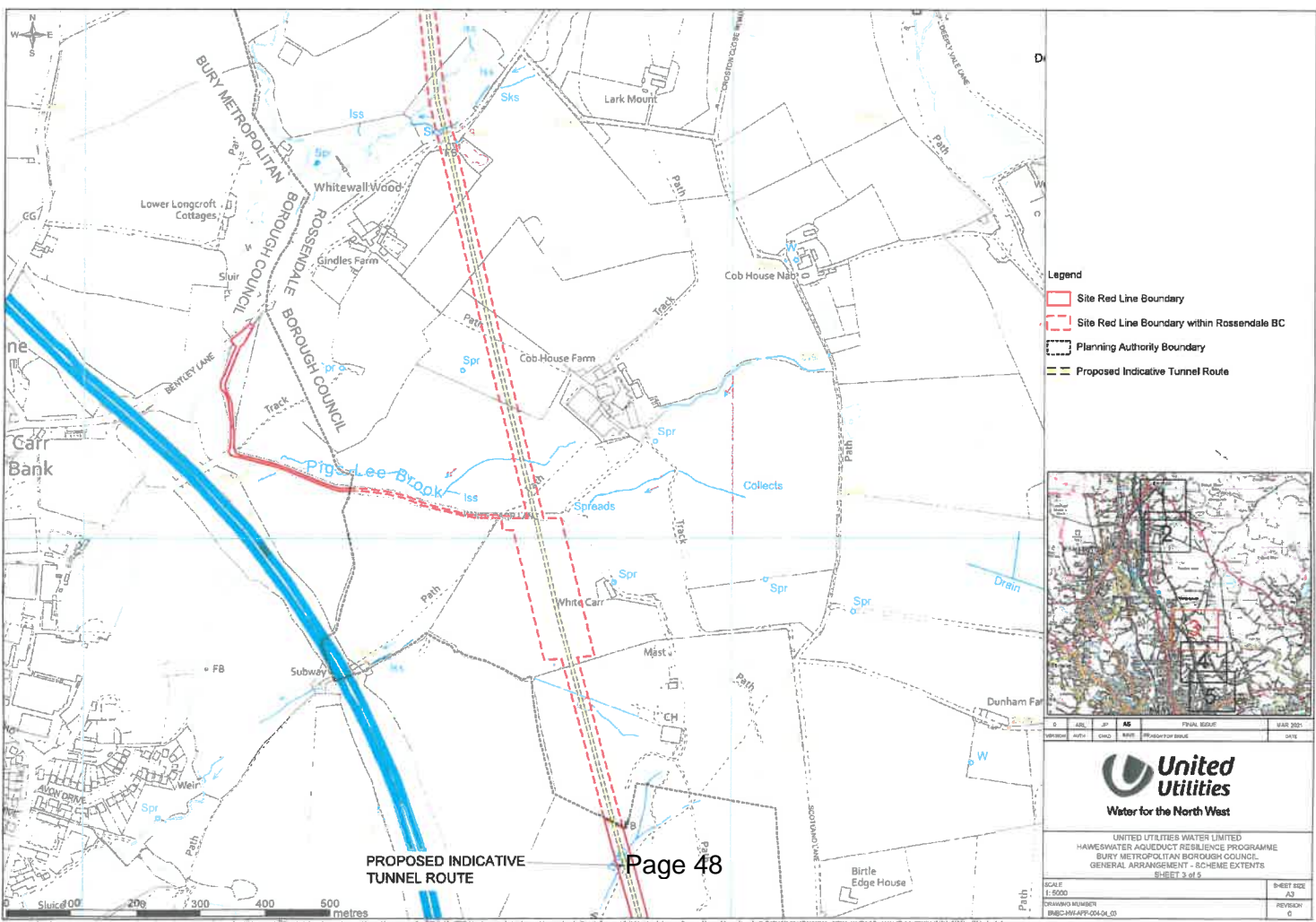
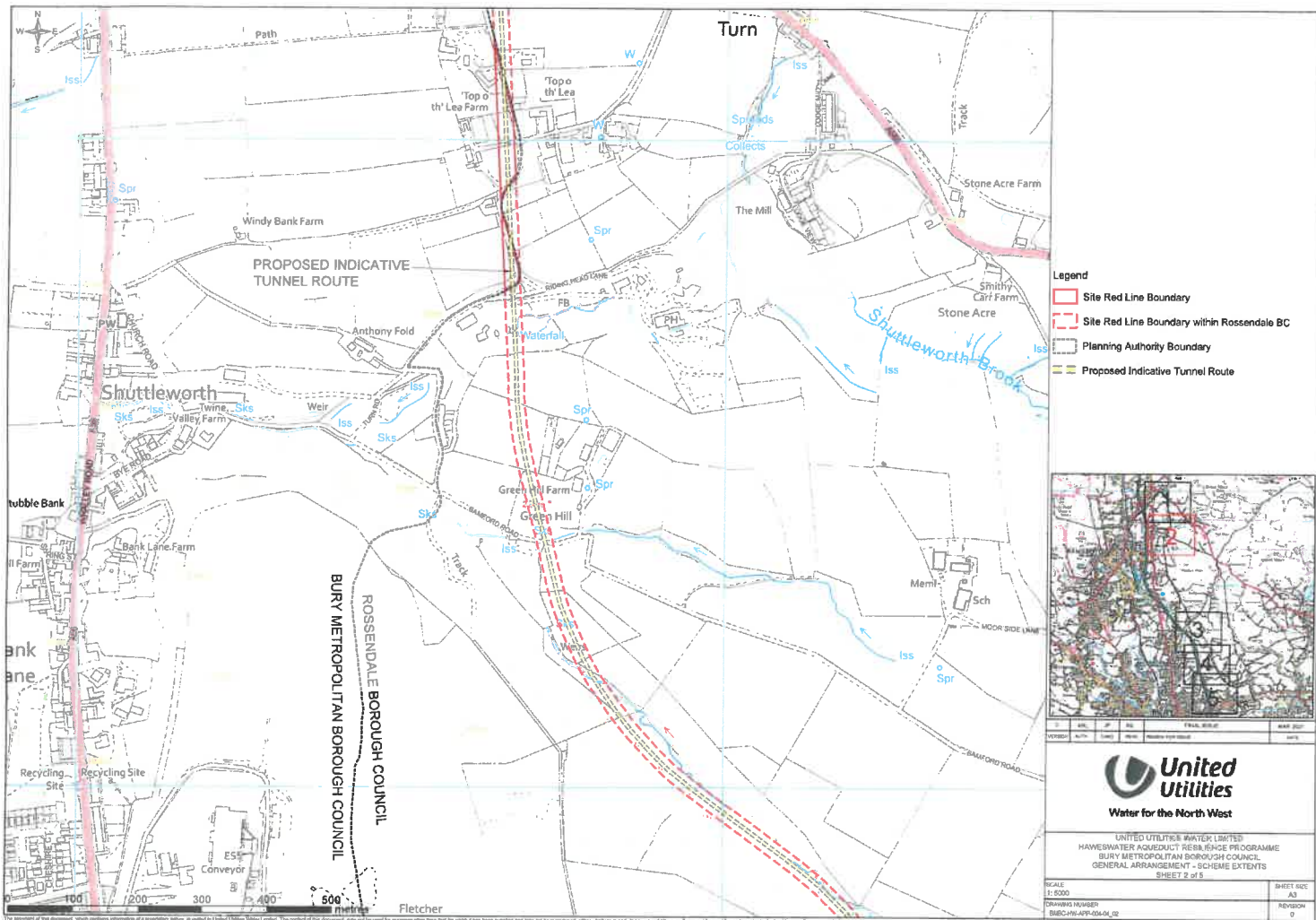








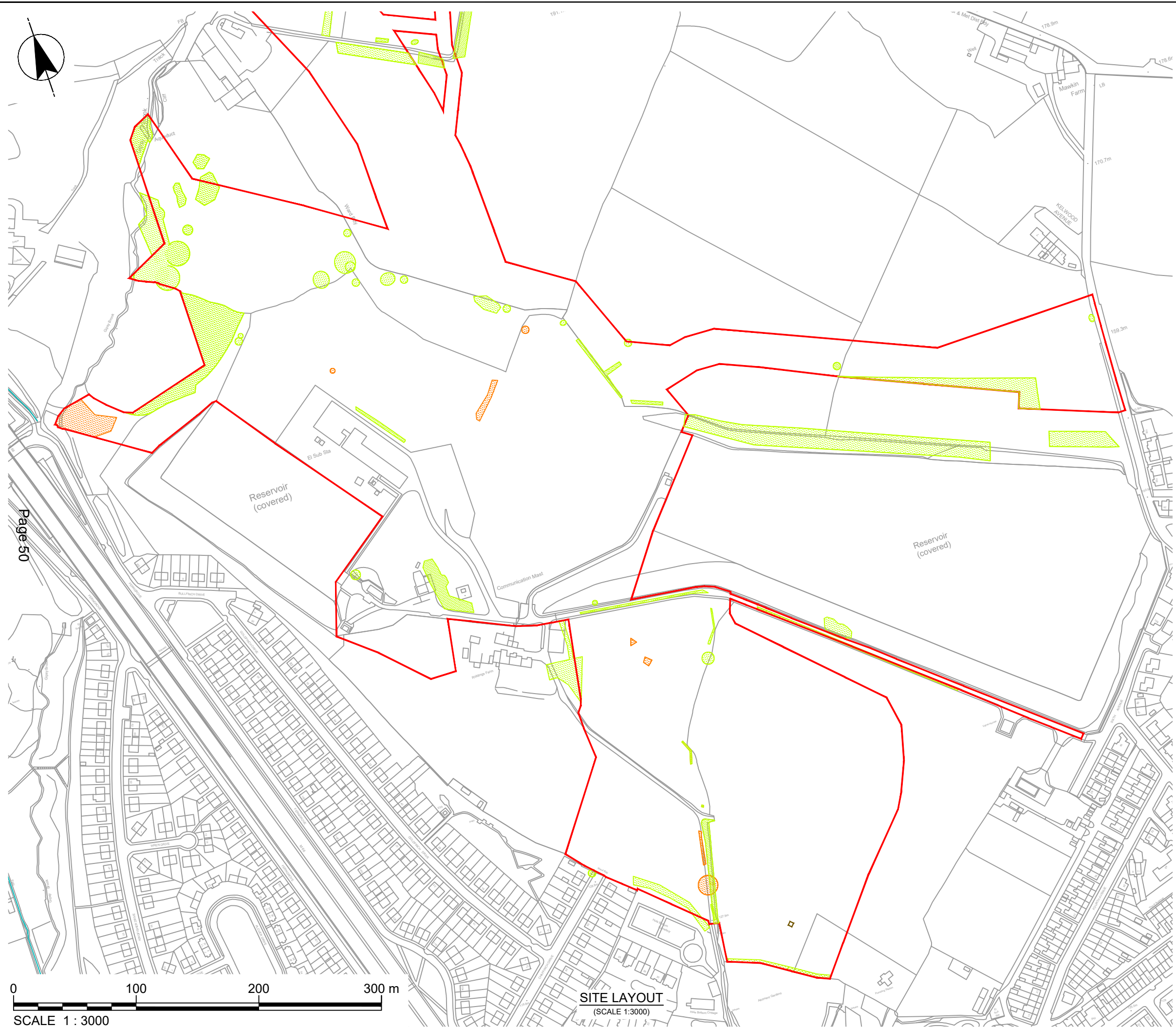












- NOTES**
1. ALL DIMENSIONS AND LEVELS IN METRES AOD UNLESS NOTED OTHERWISE.
  2. REFER TO THE ENVIRONMENTAL MASTERPLAN FOR DETAILS OF ARBORICULTURAL IMPACTS INCLUDING PROPOSED REMOVALS.

**REFERENCES**

ENVIRONMENTAL MASTERPLAN (9 AND 10 OF 10, HBC\_RBC\_BMBC-HW-FIG-020-001)

**OS MAP REFERENCE**

SD8211, SD8212

**LEGEND**

- SITE PLANNING APPLICATION BOUNDARY
- EXISTING WOODLAND/ OTHER VEGETATION
- EXISTING WOODLAND/ OTHER VEGETATION TO BE PERMANENTLY REMOVED

TO BE REPRODUCED IN COLOUR ONLY

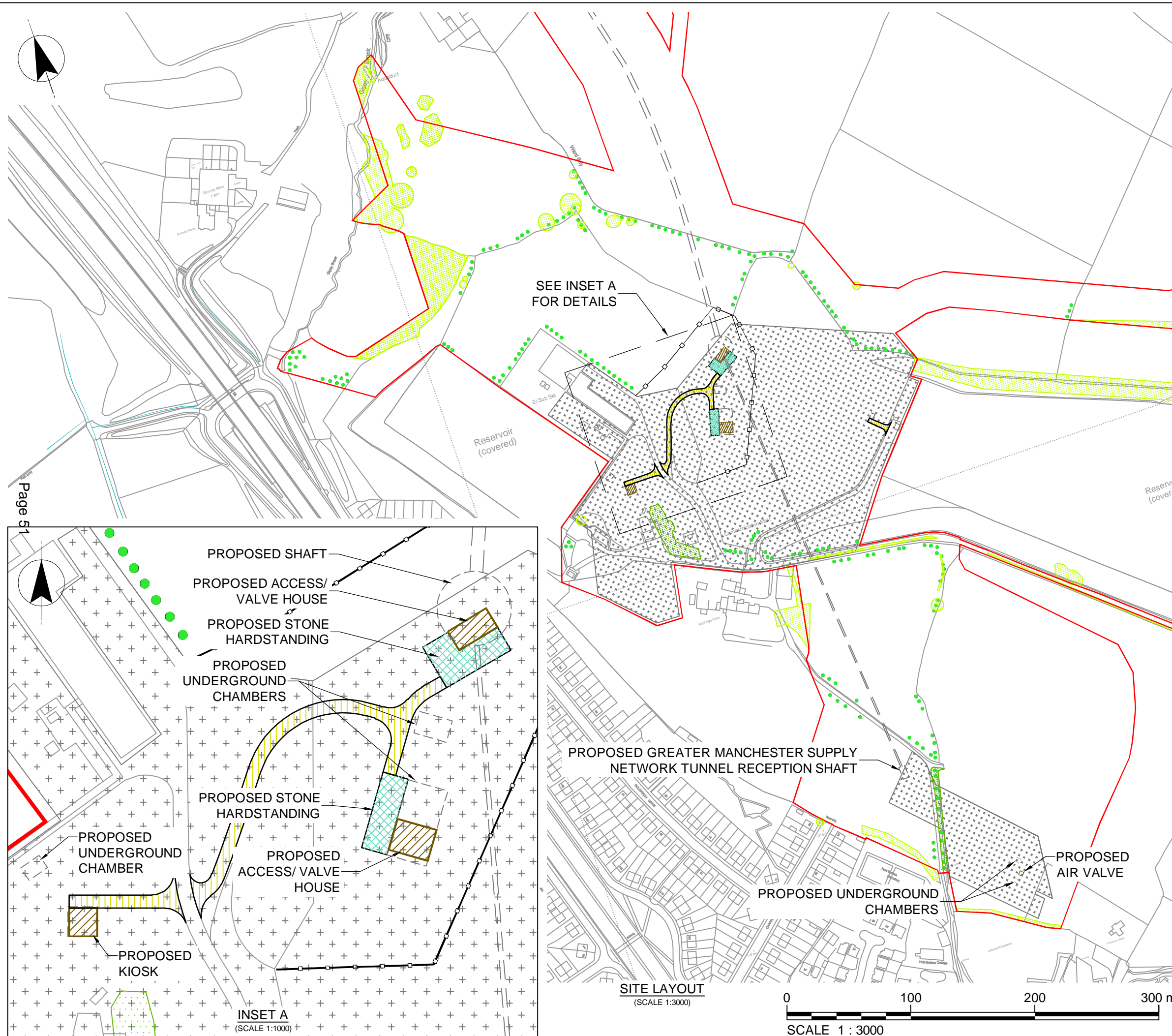
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VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE	DATE
S2				Issued for Information	
SUITABILITY CODE	SUITABILITY DESCRIPTION				



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQODUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
EXISTING SITE LAYOUT  
WOODGATE HILL WTW COMPOUND

SCALE AS SHOWN	SHEET SIZE A3
DRAWING NUMBER 80061155-01-JAC-TR5-97-DR-C-00007	REVISION P03





## NOTES

1. ALL DIMENSIONS AND LEVELS IN METRES AOD UNLESS NOTED OTHERWISE.
2. REFER TO THE ENVIRONMENTAL MASTERPLAN FOR DETAILS OF ARBORICULTURAL IMPACTS INCLUDING PROPOSED REMOVALS.
3. ALL BOUNDARY FEATURES SUCH AS HEDGEROWS, STONE WALLS AND FENCES ARE TO BE REINSTATED.

## REFERENCES

WOODGATE HILL WTW COMPOUND VALVE KIOSK  
(80061155-01-UU-TR5-97-DR-C-00042)

WOODGATE HILL WTW COMPOUND VALVE  
HOUSE AND ACCESS BUILDING (NORTH)  
(80061155-01-UU-TR5-97-DR-C-00043)

WOODGATE HILL WTW COMPOUND VALVE  
HOUSE AND ACCESS BUILDING (SOUTH)  
(80061155-01-UU-TR5-97-DR-C-00044)











TYPICAL RAISED AIR VALVE DETAIL  
(80061155-01-UU-TR5-97-DR-C-00047)

ENVIRONMENTAL MASTERPLAN (9 AND 10 OF 10,  
HBC\_RBC\_BMBC-HW-FIG-020-001)

## OS MAP REFERENCE

SD8212

### LEGEND

- |   |   |
|---|---|
|  | SITE PLANNING APPLICATION BOUNDARY                                  |
|  | PROPOSED STONED ACCESS ROAD   |
|  | PROPOSED BELOW GROUND WORKS   |
|  | PROPOSED EXTENSION TO EXISTING SECURITY FENCE                       |
|  | PROPOSED HARDSTANDING   |
|  | PROPOSED ABOVE GROUND STRUCTURES                                    |
|  | AREA TO ACCOMMODATE CONNECTION TO EXISTING UTILITIES INFRASTRUCTURE |
|  | RETAINED WOODLAND/ OTHER VEGETATION                                 |
|  | REINSTATED WOODLAND/ OTHER VEGETATION                               |
|  | PROPOSED TREE   |

TO BE REPRODUCED IN COLOUR ONLY

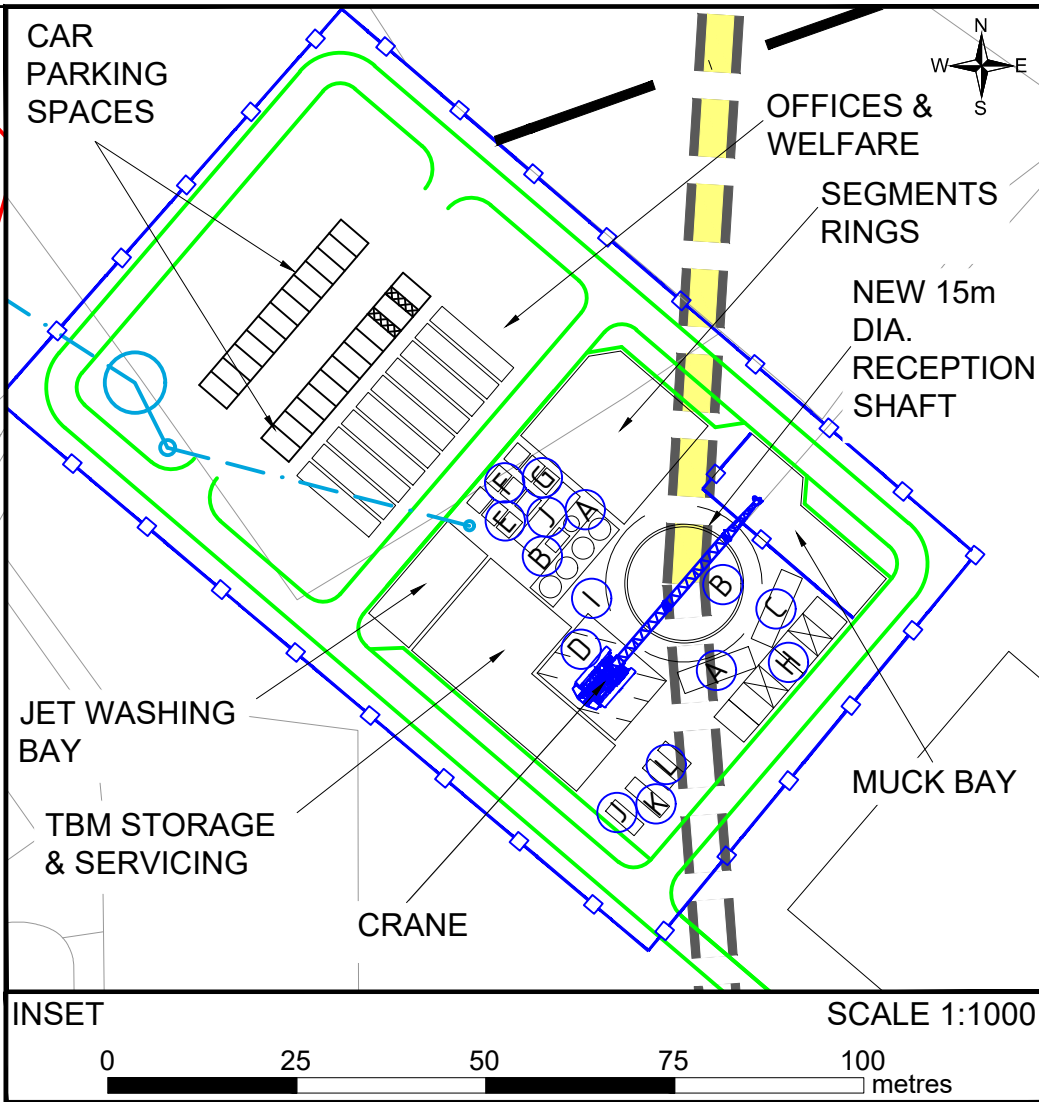
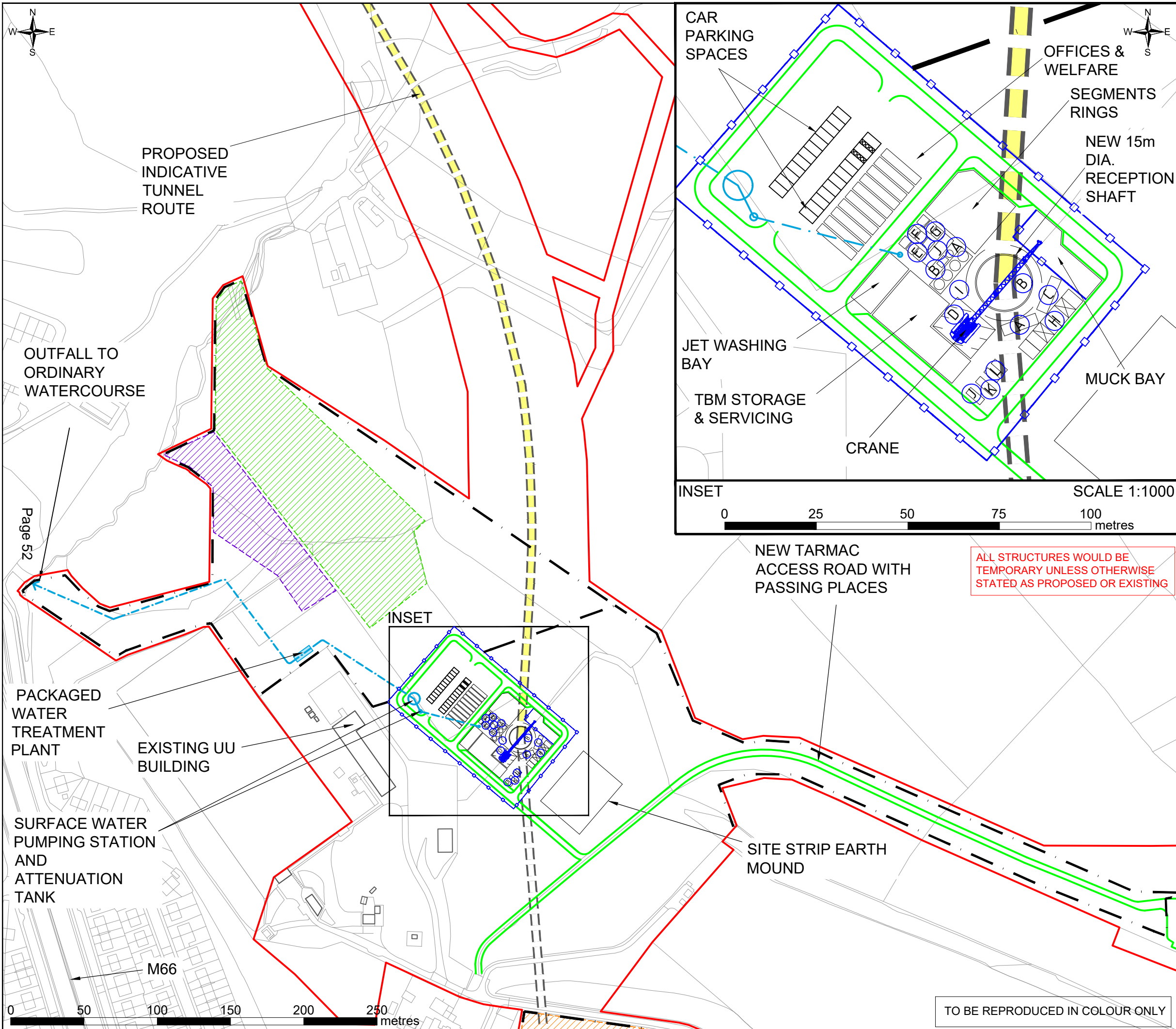
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VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE	DATE
S2		Issued for Information			
SUITABILITY CODE		SUITABILITY DESCRIPTION			



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
PROPOSED PERMANENT SITE LAYOUT  
WOODGATE HILL WTW COMPOUND

SCALE AS SHOWN	SHEET SIZE A3
DRAWING NUMBER 80061155-01-JAC-TR5-97-DR-C-00008	REVISION P01





Notes

Indicative alignment of heras fencing to demarcate active working areas within the compound and shown as a worst case scenario, as alignment likely to change during the works. Acoustic screening to be provided by Contractor during noisy works within overall site extents.

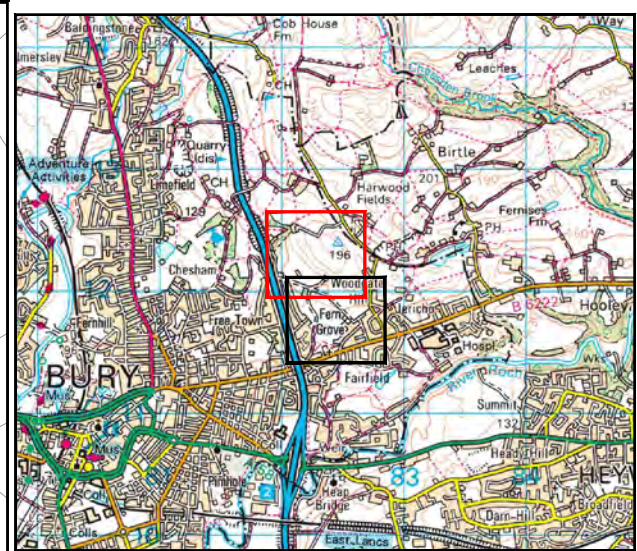
For details of mine grouting area works please refer to drawings ref : BMBC-HW-APP-004-011\_01 to 03.

Refer to the Environmental Masterplan within the Environmental Statement (Figure HBC\_RBC\_BMBC\_HW-FIG-020-001) for environmental protection and mitigation measures, and any proposed temporary closures and/or diversions to PRoW.

Legend

- Site red line boundary
- 2.4m Hoarding fence
- 2m Temporary fencing (Heras type)
- Access to existing UU infrastructure for potential remedial works when taken out of service
- Proposed alternative surface water outfall location (if required)
- Potential storage area (if required)

A	Vent	G	COSHH stores
B	15m dia. shaft	H	Workshops / stores
C	Tally hut	I	Grout batching
D	Crane pad	J	Diesel fuel tank
E	Skips	K	Transformer
F	Oil grease	L	KVA generator



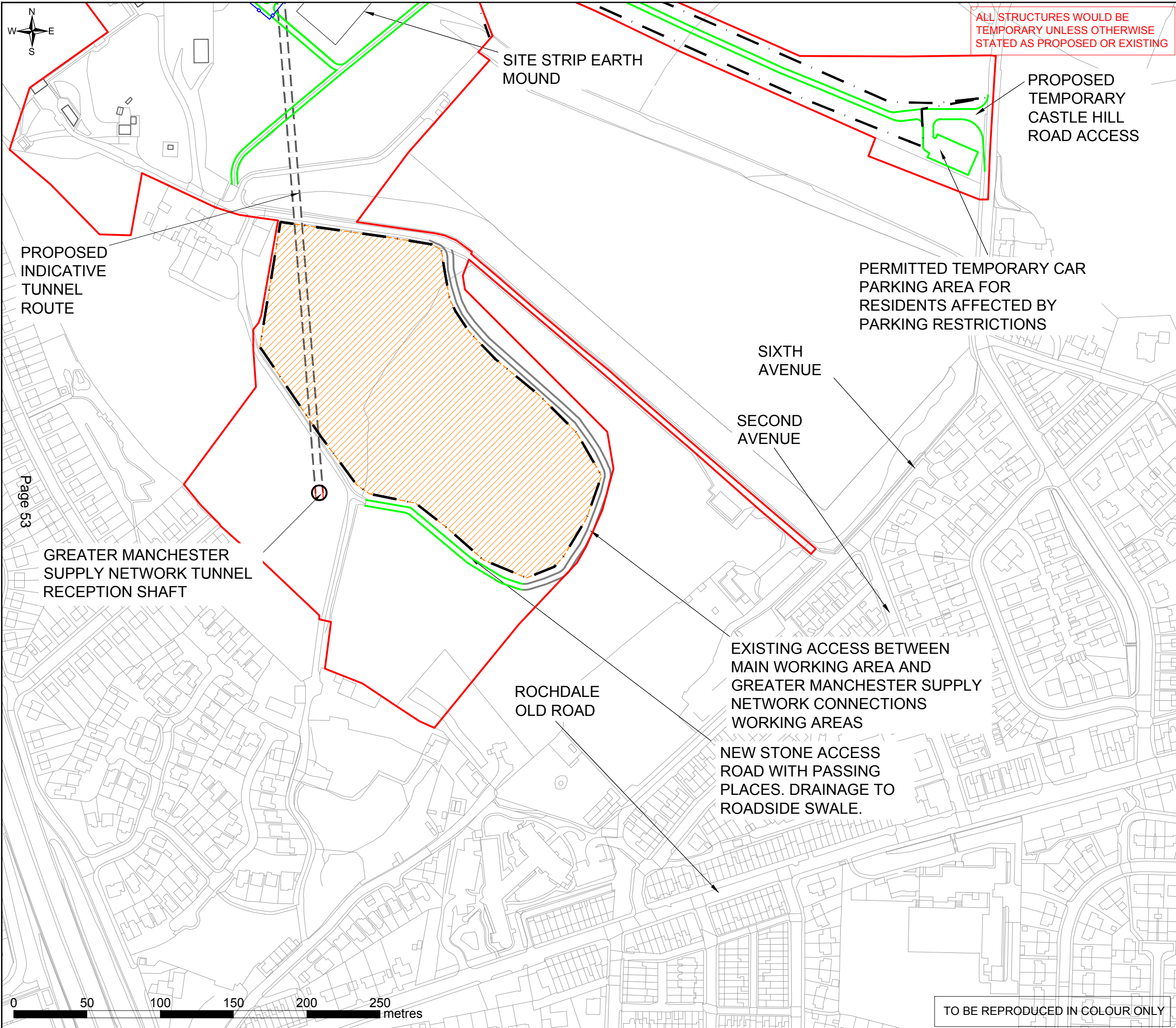
0	JS	JP	AS	FINAL ISSUE	APR 2021
VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE	DATE

**United Utilities**  
Water for the North West

UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
PROPOSED WOODGATE HILL WTW COMPOUND  
CONSTRUCTION PHASE DRAWING SHEET 1 OF 2

SCALE 1: 2,500	SHEET SIZE A3
DRAWING NUMBER BMBC-HW-APP-004-05_01	REVISION 0





**Notes**

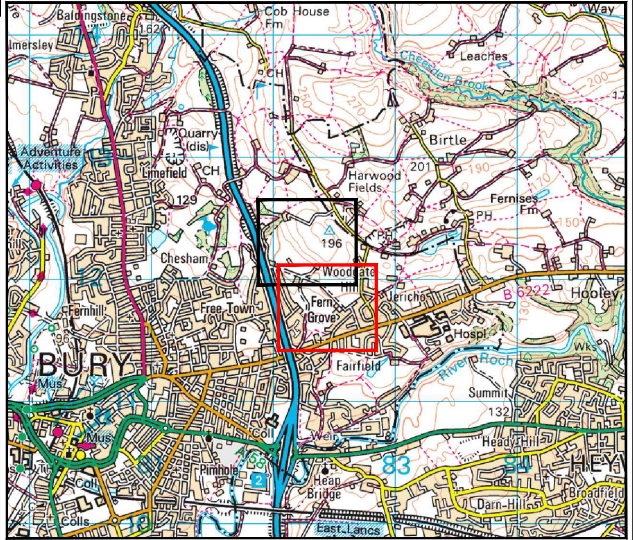
Indicative alignment of heras fencing to demarcate active working areas within the compound and shown as a worst case scenario, as alignment likely to change during the works. Acoustic screening to be provided by Contractor during noisy works within overall site extents.

For details of mine grouting area works please refer to drawings ref : BMBC-HW-APP-004-011\_01 to 03.

For Compound Access Details refer to drawing BMBC-HW-APP-004-11\_01.

Refer to the Environmental Masterplan within the Environmental Statement [Figure HBC\_RBC\_BMBC\_HW-FIG-020-001], for environmental protection and mitigation measures and any proposed temporary closures and/or diversions to PRow.

- Legend**
- Site red line boundary
  - 2.4m Hoarding fence
  - 2m Temporary fencing (Heras type)
  - Potential storage area (if required)



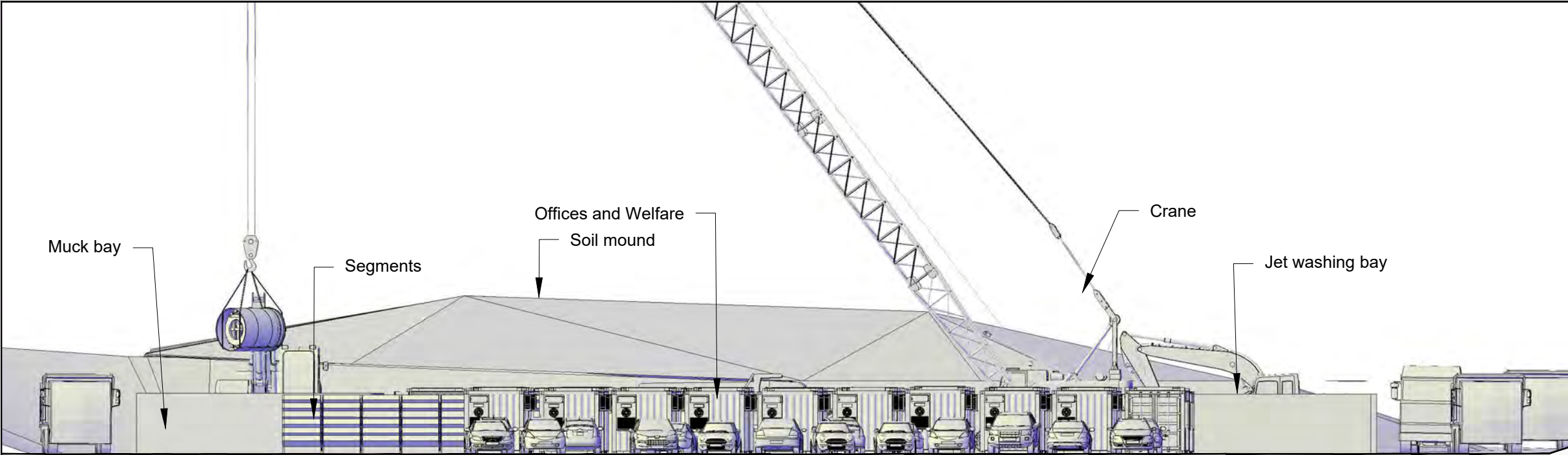
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0	JS	JP	AS	FINAL ISSUE	APR 2021
VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE	DATE



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
PROPOSED WOODGATE HILL WTW COMPOUND  
CONSTRUCTION PHASE DRAWING SHEET 2 OF 2

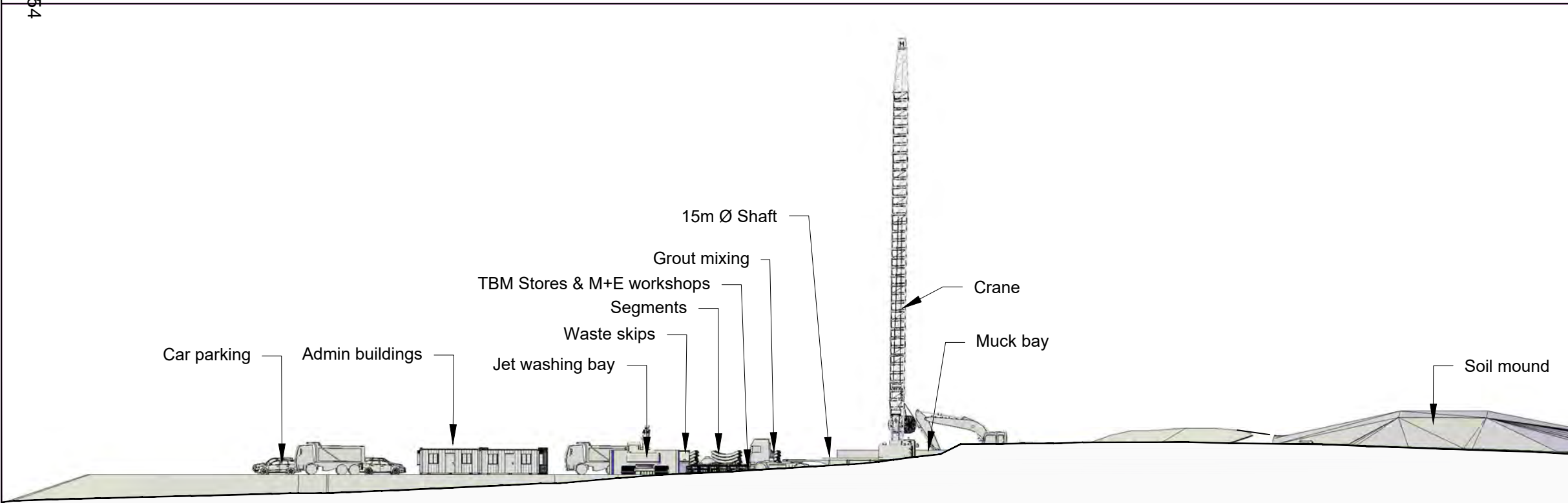
SCALE 1: 2,500	SHEET SIZE A3
DRAWING NUMBER BMBC-HW-APP-004-05_02	REVISION 1





INDICATIVE ELEVATION A-A - APPROX 1:200@A3

Page 54



INDICATIVE ELEVATION B-B - APPROX 1:500@A3

Notes

Elevation details shown reflect temporary compound and construction activities.

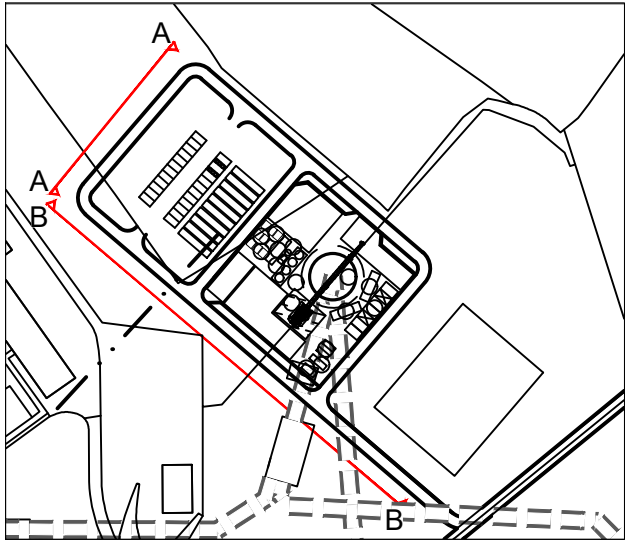
Indicative elevations with the hoarding and fencing removed to allow a view of the compound.

For detailed dimensions refer to drawing  
BMBC-HW-APP-004-07 - Cross Sections Woodgate Hill WTW Compound

For Construction Compound Details refer to drawing  
BMBC-HW-APP-004-05  
PROPOSED WOODGATE HILL WTW COMPOUND - CONSTRUCTION PHASE DRAWING

Refer to the Environmental Masterplan within the Environmental Statement [Figure HBC\_RBC\_BMBC\_HW-FIG-020-001], for environmental protection and mitigation measures, and any proposed temporary closures and/or diversions to PRoW.

TO BE REPRODUCED IN COLOUR ONLY

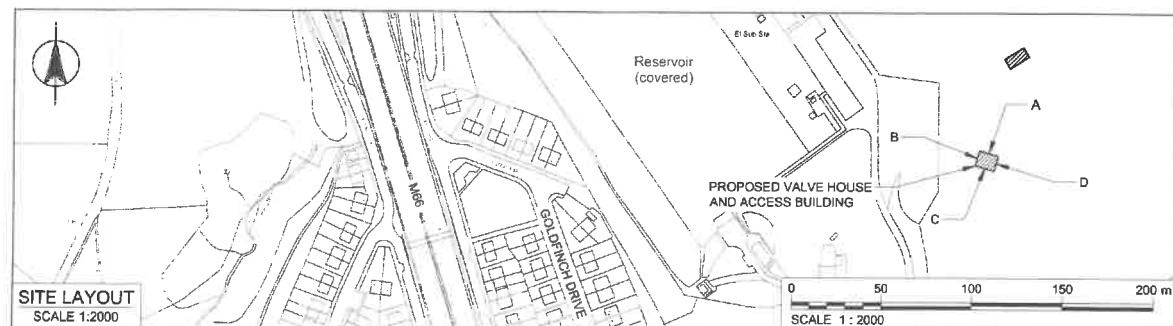


0	RL	JP	AS	FINAL ISSUE		APR 2021
VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE		DATE



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
COMPOUND ELEVATIONS - WOODGATE HILL WTW COMPOUND

SCALE AS SHOWN		SHEET SIZE A3
DRAWING NUMBER BMBC-HW-APP-004-09		REVISION 0



#### NOTES

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#### KEY TO EXTERNAL MATERIALS:

ROOF: PROFILED CLADDING MERLIN GREY RAL 180 40 05 TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

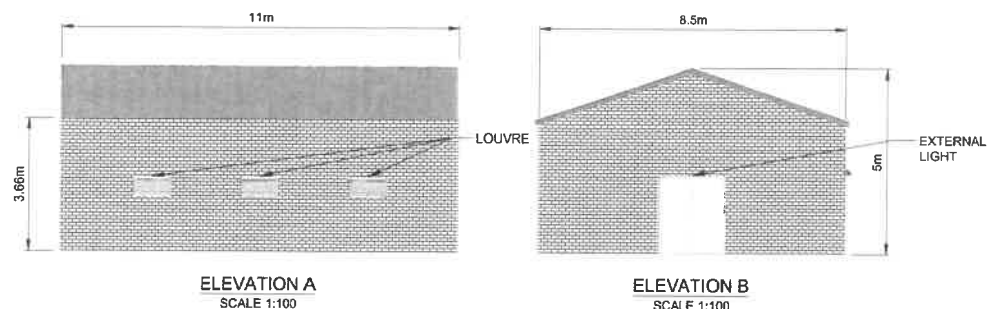
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DOORS: STEEL COLOUR GRAPHITE GREY RAL7024 TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

LOUVRES: STEEL OR ALUMINIUM COLOUR GRAPHITE GREY RAL7024 TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

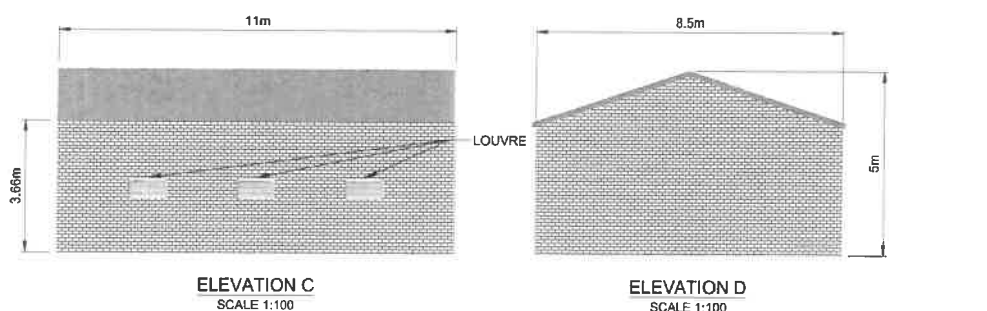
#### LEGEND

ROOF CLADDING



ELEVATION A  
SCALE 1:100

ELEVATION B  
SCALE 1:100

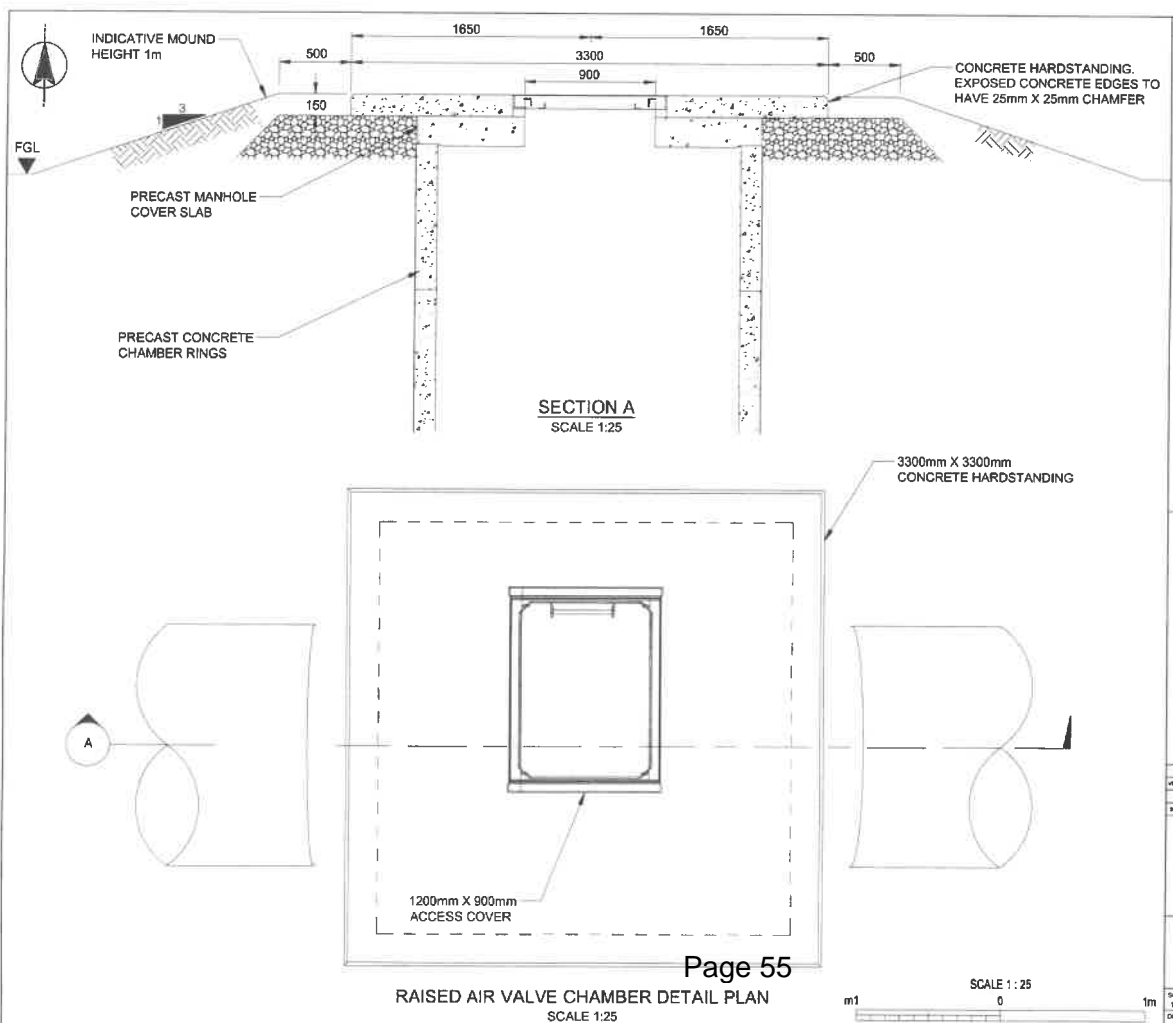


ELEVATION C  
SCALE 1:100

ELEVATION D  
SCALE 1:100

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PD2	ER	JT	OH	For Information	09/04/21
VERSION	4/21	CHD	REV	REASON FOR ISSUE	DATE
50				Initial Status or WIP	
BUTTERFLY CODE BUTTERFLY DESCRIPTION					
UNITED UTILITIES WATER LIMITED HAWESWATER ACADEMIST RESILIENCE PROGRAMME BURY METROPOLITAN BOROUGH COUNCIL WOODGATE HILL WTW COMPOUND VALVE HOUSE AND ACCESS BUILDING (SOUTH)					
SCALE AS SHOWN					SHEET SIZE A3
DRAWING NUMBER 80051155-01-UU-TRS-97-DR-C-00041					REVISION PD2

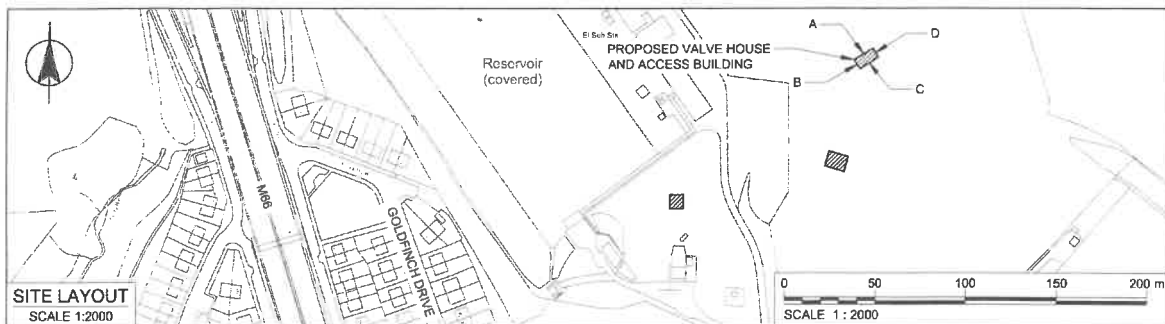


#### NOTES

1. ALL DIMENSIONS IN MILLIMETRES AND ALL LEVELS IN METRES ADD UNLESS NOTED OTHERWISE.
2. NO DIMENSIONS ARE TO BE SCALED FROM THIS DRAWING.

PD1	ER	JT	OH	For Information	09/04/21
VERSION	4/21	CHD	REV	REASON FOR ISSUE	DATE
50				Initial Status or WIP	
BUTTERFLY CODE BUTTERFLY DESCRIPTION					
UNITED UTILITIES WATER LIMITED HAWESWATER ACADEMIST RESILIENCE PROGRAMME BURY METROPOLITAN BOROUGH COUNCIL TYPICAL RAISED AIR VALVE CHAMBER DETAIL					
SCALE 1:25					SHEET SIZE A3
DRAWING NUMBER 80051155-01-UU-TRS-97-DR-C-00047					REVISION PD1

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#### KEY TO EXTERNAL MATERIALS:

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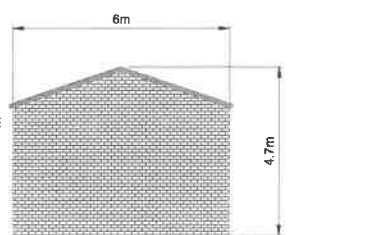
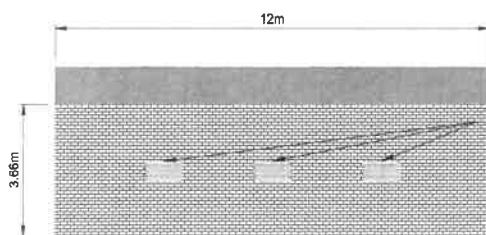
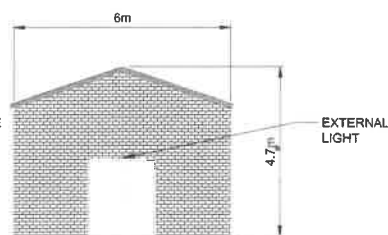
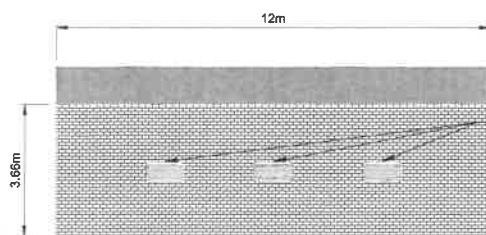
WALLS: COURSED BRICKWORK TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

DOORS: STEEL COLOUR GRAPHITE GREY RAL7024 TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

LOUVRES: STEEL OR ALUMINIUM COLOUR GRAPHITE GREY RAL7024 TO MATCH ADJACENT UU OPERATIONAL BUILDINGS.

#### LEGEND

ROOF CLADDING



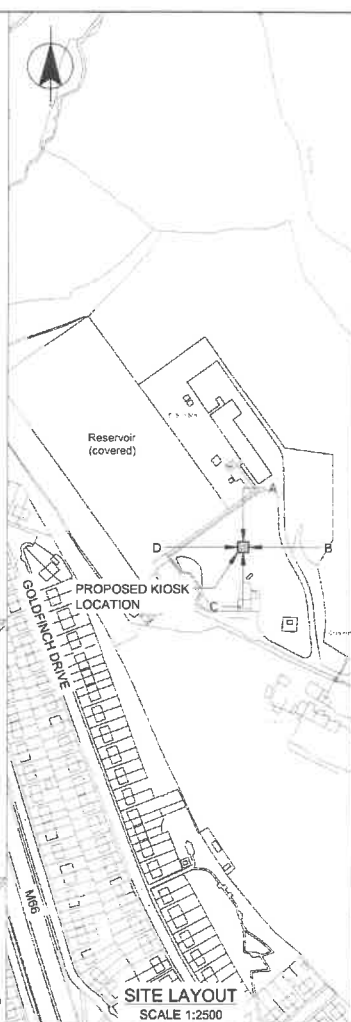
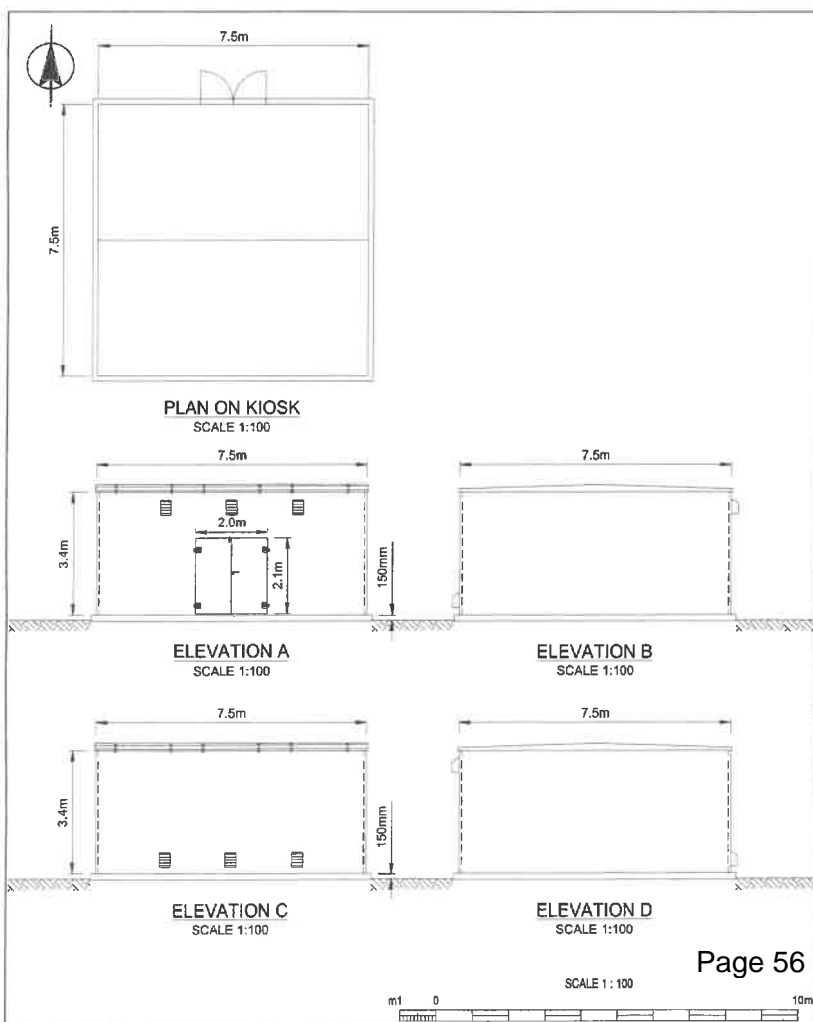
PC2	ER	JT	CH	For Information	05/04/21
VERSION	ALPH	CHD	REQD	REASON FOR ISSUE	DATE
SO				Initial Status or WSP	
SUPPLY CODE				SUPPLY DESCRIPTION	



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
WOODGATE HILL WTW COMPOUND  
VALVE HOUSE AND ACCESS BUILDING (NORTH)

SCALE	SHEET SIZE
AS SHOWN	A3
DRAWING NUMBER	REVISION
80081155-01-UU-TR5-07-DR-C-00043	PC2

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#### LEGEND

VENTILATION LOUVRE

PC2	ER	JT	CH	For Information	05/04/21
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SO				Initial Status or WSP	
SUPPLY CODE				SUPPLY DESCRIPTION	

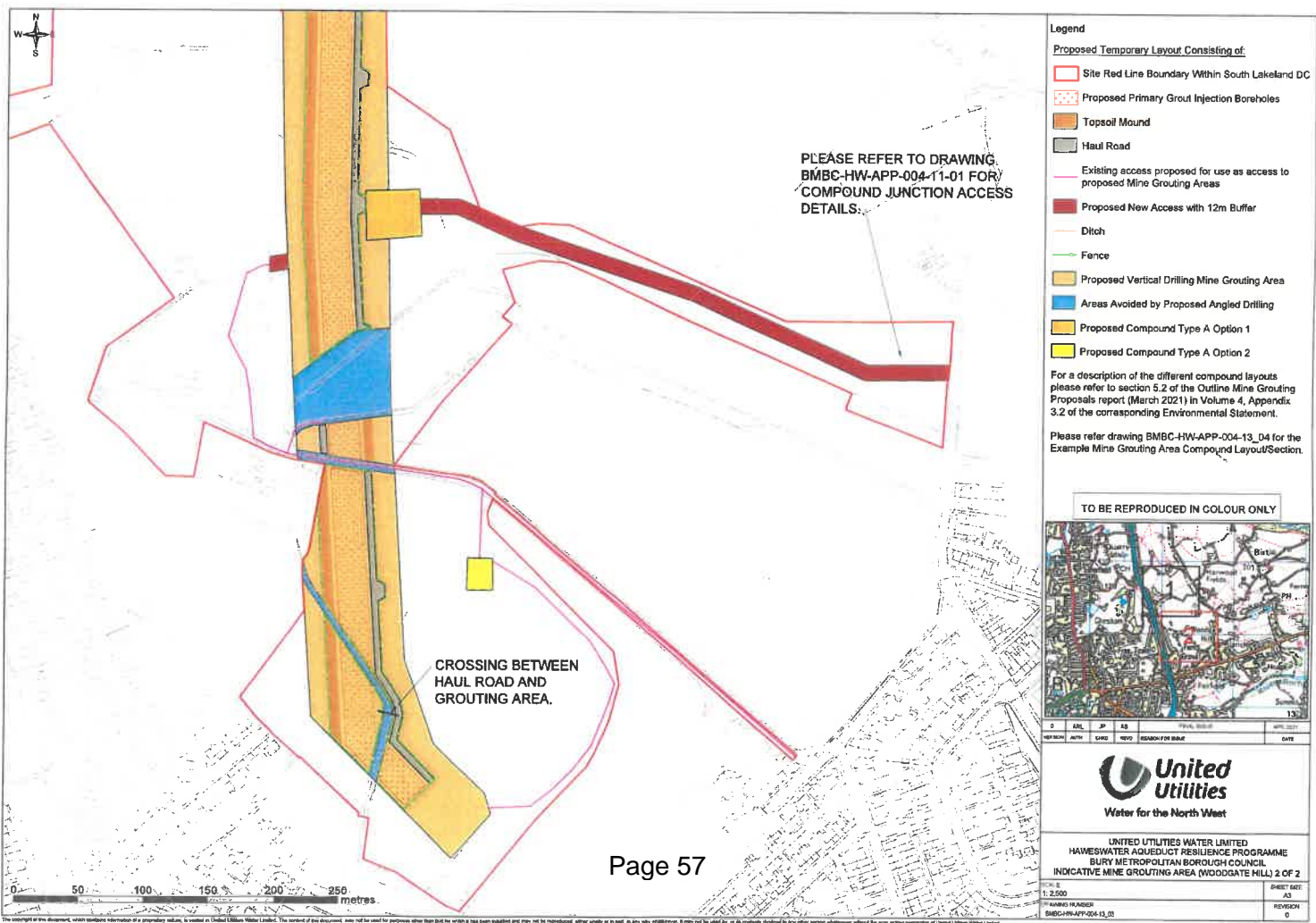
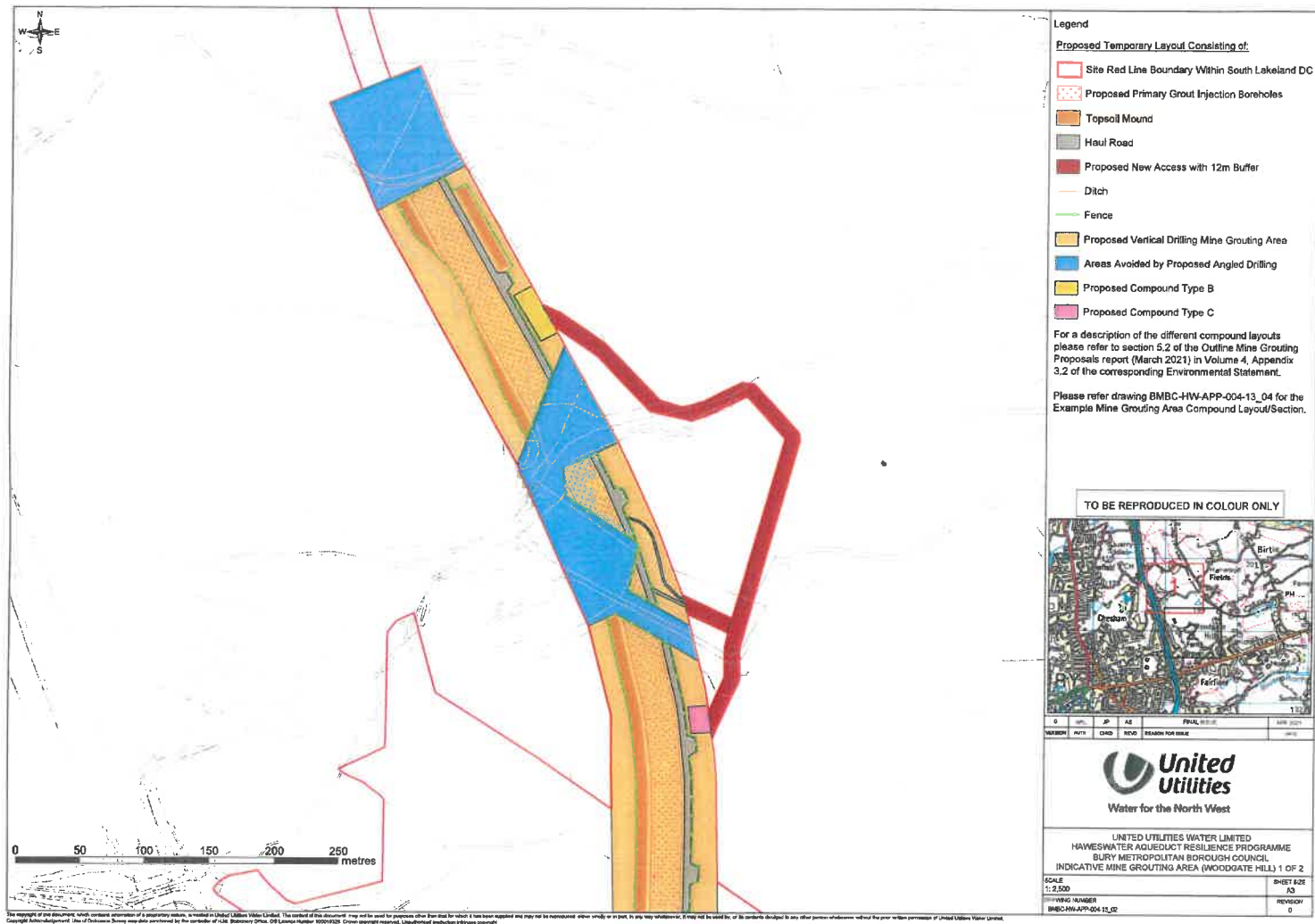


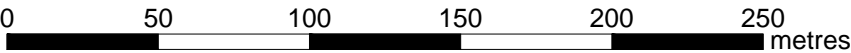
UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
BURY METROPOLITAN BOROUGH COUNCIL  
WOODGATE HILL WTW COMPOUND  
VALVE KIOSK

SCALE	AS SHOWN	SHEET SIZE
DRAWING NUMBER	80091155-01-04-TR5-07-DR-C-00043	REVISION
		PC2

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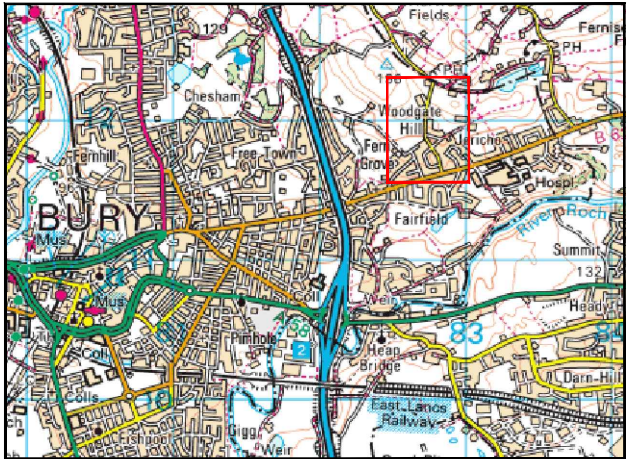






Legend

- PROPOSED KERB
- VISIBILITY SPLAY (INCLUDING VEGETATION CLEARANCE WHERE NECESSARY)
- EXISTING LIGHTING COLUMN TO BE RELOCATED
- EXISTING STONE WALL TO BE REMOVED
- EXISTING KERB TO BE REMOVED
- PROPOSED PEDESTRIAN ACCESS
- PROPOSED PEDESTRIAN FOOTWAY
- UNCONTROLLED CROSSING TACTILE PAVEMENT



1	HS	SP	SP	FIRST DRAFT	OCT 2021
VERSION	AUTH	CHKD	REVD	REASON FOR ISSUE	DATE



UNITED UTILITIES WATER LIMITED  
HAWESWATER AQUEDUCT RESILIENCE PROGRAMME  
COMPOUND JUNCTION ACCESS DETAILS  
WOODGATE HILL WTW COMPOUND

SCALE 1:1000	SHEET SIZE A3
DRAWING NUMBER BMBC-HW-APP-004-11_01	REVISION 1

**Ward:** Radcliffe - East

**Item 2**

**Applicant:** Great Places Housing Group

**Location:** East Lancs Paper site, land between Cockclod Street/Cunliffe Street/Howard Street/Pollitt Street, Radcliffe Manchester, M26 9PG

**Proposal:** Residential development of 27 no. houses and apartments with car parking, landscaped areas and associated works

**Application Ref:** 67965/Full

**Target Date:** 20/04/2022

**Recommendation:** Approve with Conditions

### **Description**

The application site extends to approximately 1.78 acres (0.72 ha) and was formerly part of the site of the former East Lancashire Paper Mill, which has now been demolished.

The site presently consists of connecting highways and an area of grassland, which is bounded by terraced residential properties which either back on to or front on to the area.

The site includes several distinct areas including areas with grass, trees and the remains of a car park accessed from Rectory Lane. The site is generally level.

The northern area of the site bound by 4 streets, Spring Lane, Cock Clod Street, Cunliffe Street & Pollitt Street. Palisade fencing bounds large portions of this area of the site and includes a Sub station. Vegetation has started to reclaim this area of site.

The site is located on the southern edge of the settlement of Radcliffe, with the main urban area being located to the north and west of the site. The town is mostly characterised by terraced and semi-detached houses.

Radcliffe is the nearest town centre and is easily accessible by walking, cycling, bus or car. The site is within a very short walk of Radcliffe Metrolink station providing connections to Manchester city centre, Bury and Altrincham which are all connected to the wider the Greater Manchester and National transport networks.

The site is identified as being within East Radcliffe on the Bury Proposals Map. Parts of the site lie within Flood Zone 2 and Flood Zone 3 as identified on the Environment Agency's mapping website. Part of the site is allocated as Protected Recreation Provision in the Urban Area.

The application seeks planning permission for 27 residential units. The development comprises two parts. A row of 7 terraced properties and an apartment block of 20 units. The terraced houses would contain -

- 5no. three bedroom houses,
- 2no. four bedroom houses with rooms in the roofspace

The apartment block would contain -

- 17no. one bedroom apartments
- 3no. two bedroom apartments.

Elevationally the dwellings would be contemporary in appearance with a post-industrial appearance comprising brick and tiled roof with keynote oriel type windows in the front elevation and roof lights. The rear having a more straightforward appearance. Long gardens

behind the units would be provided and each property providing two parking spaces (14 no in total).

The apartment block would similarly maintain an industrial appearance with its main access located on to Porritt Street. An area of open space would be provided to the north. Car parking for the apartment block would comprise a pair of parallel on-street spaces on Pollitt Street, 3 no parallel spaces on Cunliffe Street and a small car park of 9 spaces.

All of the properties would be affordable dwellings.

### **Relevant Planning History**

55901 - Application to extend the time limit by 3 years for implementation of planning permission 45598 for hybrid outline application for mixed use development comprising Class B1, B2, B8, industry, residential development, water feature, open space and full application for secondary school and highway infrastructure. The extension of time requested relates solely to the outline element and condition no. 2 attached to the original permission - Approve with Conditions 04/07/2013

57237 - Temporary contractor's compound in association with permitted drainage works - Withdrawn - Invalid 10/03/2014

The site forms part of the overall Masterplan for the wider ELPM site. The planning history for the site includes:

Ref. 67423 - 2021 - Variation of condition 39 following approval of planning permission 62969 - relocate the vehicular access shown on the approved plan A090070-193-P002 Revision P03, two metres to the East as listed within Condition 39. Approved with conditions.

Ref. 67597 - 2021 - Variation to conditions 2 for consistency and to allow phasing; 3 removing indicative masterplan from list of approved plans; 8 and 42 for consistency; 9, 23, 33 and 39 to amend the trigger point for submitting information; 11 to allow site clearance.

Ref. 62969 - 2018 - O/L 400 dwellings. Approved with conditions.

Ref. 55901 - 2013 - Application to extend the time limit by 3 years for implementation of planning permission 45598 for hybrid outline application for mixed use development comprising Class B1, B2, B8, industry, residential development, water feature, open space. - Approved with conditions.

Ref. 45598 - 2007 - Hybrid Outline Application for mixed use development comprising class B1, B2, B8, industry, residential development, water feature, open space and Full Application for secondary school and highway infrastructure.

### **Publicity**

106 addresses were notified by letter sent 26/1/22. Site notice placed 28/1/22 and a press notice published in the Bury Times on 3/2/22.

As a result of this publicity, 4 objections in total have been received, 3 objections received from residents and Cllr Birchmore.

Residents objections -

- Understand the land where the houses are being built was gifted to the residents by Bury Council, how can this have been sold? The green space is a nice sunny area that the residents and children use, why not put a park there instead and build on the old pub car park?



- Object to the entrance to the apartments or car park be from Pollitt Street or accessed via Howard Street.
- The road is full of potholes and getting out at the end of Howard Street is already difficult enough due to parked cars blocking visibility. For years we have enjoyed a quiet street with just 3 houses and the children play on the concreted area as some of the houses have no gardens. We are a 3 car household so there is already limited space.
- We also have a number of 1 bed flats that have just been built across the road by Bolton at Home. My understanding from the original plans that it was to be 3 houses. I will be looking to sell up happy to consider an offer from the developer
- Object on the basis there are enough houses and traffic around this area already
- The amount of parking allocated for the new developments is inappropriate compared to the number of properties and expected residents. With the lack of parking spaces for the new development and no consideration for existing residents there will be more cars on the side of the road and therefore the surrounding areas and roads. This will create a safety hazard especially for children. Parking must be addressed for new and existing residents prior to the development going ahead. I was informed the original plans from the developer had more appropriate parking so clearly even the developer recognised the issue. Safety of residents and children of the area should be a priority.

1 objection from Councillor Birchmore -

- Parking - Lack of designated parking - the majority of people are reliant on their cars
- Disabled parking provision - Good to see the ground floor flats designed with disability access in mind but there should be a minimum of 1 space per ground floor flat. More disabled parking would reduce availability for other flats.
- Lack of parking would lead to parking disputes - there should be designated spaces.
- EV charge points - The developer should provide a vehicle chargepoint for all spaces.
- Ensure existing houses are not subject to loss of light or privacy and consideration given to window placement to ensure privacy is not compromised - measures to mitigate by tree cover could be considered.
- The road layout and entrance/exit visibility should be carefully considered to ensure that a potentially dangerous road situation due to the increased traffic volumes entering and exiting Cockclod Street is avoided.

Respondents have been notified of the Planning Control Committee meeting.

### **Statutory/Non-Statutory Consultations**

**Traffic Section** - No objections in principle subject to conditions which will be reported in the Supplementary Agenda.

**Environmental Health - Contaminated Land** - No objections subject to conditions

**Waste Management** - No objections

**Borough Engineer - Drainage Section** - No response

**Environment Agency** - No objections subject to conditions

**Greater Manchester Ecology Unit** - No significant ecological issues. Issues relating to bats, nesting birds, invasive species and impacts on the watercourse and landscaping can be resolved via condition.

**United Utilities (Water and waste)** - No objection subject to condition.

**Greater Manchester Police - designforsecurity** - No response received

**The Coal Authority** - Low risk - No objections

**Environmental Health - Pollution Control** - No objections

**Pre-start Conditions** - Applicant/Agent has agreed with pre-start conditions

### **Unitary Development Plan and Policies**

NPPF      National Planning Policy Framework

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H4/1	Affordable Housing
H5/1	Area Improvement
EC2/1	Employment Generating Areas
EN1/2	Townscape and Built Design
EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN7	Pollution Control
RT1/1	Protection of Recreation Provision in the Urban Area
RT2/2	Recreation Provision in New Housing Development
HT2/4	Car Parking and New Development
HT5/1	Access For Those with Special Needs
HT6/1	Pedestrian and Cyclist Movement
HT6/2	Pedestrian/Vehicular Conflict
SPD1	Open Space, Sport and Recreation Provision
SPD5	DC Policy Guidance Note 5: Affordable Housing
SPD6	Supplementary Planning Document 6: Alterations & Extensions

## Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

### Principle

The site is currently allocated within the UDP as protected recreational space within the urban area. Whilst the proposal would lead to the loss of some of the RT1/1 land (circa 0.24 ha.), the application proposes to upgrade a small part of the RT1/1 land and an adjacent vacant site through the creation of a more attractive, usable and accessible area of public space containing trees, planting and seating areas (circa 0.14 ha.). The application also proposes to create an area of new open space to the north of the site alongside Spring Lane (circa 0.03 ha.). This would mean that there would be a net loss of 0.07 ha. in the quantity of open space.

However, this application site forms part of the wider East Lancs Paper Mill site which has outline planning approval for 400 homes. Detailed designs for the wider site are ongoing but there will be a significant over provision of public open space provided as part of the wider site which will be delivered by Homes England. This will include formal play provision, open space and extensive tree planting.

On balance, therefore, it is considered that the loss of the existing recreation space will be offset by better recreation provision in terms of both quantity (on application site and wider site) and quality in accordance with paragraph 99(b) of the NPPF.

The site is also surrounded by residential development and there is pre-existing infrastructure which can be utilised and upgraded by the development. As such the use would assimilate into the immediate area and would therefore be no conflict in land use terms.

### **Policy RT2/2 - Recreation Provision in New Housing**

Policy RT2/2 requires all proposals for new housing consisting of 10 units or more to make provision for the recreational needs of prospective residents.

SPD 1 provides advice in relation to developer contribution towards the provision, enhancement and maintenance of open spaces in Bury. The priority of SPD 1 is to secure off-site contributions although there may be some circumstances where on-site provision may be preferable in exceptional circumstances and were justified by the individual merits of the site.

As previously discussed, this application forms part of the wider East Lancs Paper Mill site. Within the wider site, there is the opportunity to provide a significant recreational resource for both existing communities and future prospective residents. As a stakeholder in the wider site, the Council can ensure delivery of these facilities and ensure they are accessible to the general public at all times.

On balance therefore, in this instance the Council would be prepared to accept provision of new recreational facilities within the wider site in lieu of a financial contribution.

If approved, the application should be conditioned to ensure the proper and permanent management of the proposed recreation provision and ensure the provision remains publicly accessible.

**Affordable Housing** - The proposed development provides fully affordable accommodation, with a mix of affordable rent and social rent proposed, to be managed long term by Great Places which would make a valuable contribution to the provision of local housing stock in the area. This would be secured by a planning condition.

### **Policy EC2/1/12 - Radcliffe East Employment Generating Area**

A small part of the site is currently designated as an Employment Generating Area (EGA) in the adopted Bury UDP. However previous approvals on the East Lancashire Paper Mill site concluded there is no reasonable prospect of the site being used for its allocated employment use and residential development would be accepted in principle and would not conflict with Policy EC2/1.

### **Pre-application Consultation**

The applicant submitted an AEW design document as part of a pre-application enquiry to Bury Council planning department with a view to encourage dialogue on the proposed scheme. Planning officers at Bury Council reviewed the proposals and a meeting was held between the design team, client, the Environment Agency and Bury Council planning department. A formal response from the council was provided on the proposed design in a letter dated 15th of April 2021.

Following the meeting and receipt of this letter the design team carefully considered the feedback and through design development revised the scheme. Subsequent meetings were held with the planners and wider consultees including the EA Consultation with the planners throughout the development of the scheme has been extensive.

### **Statement of Community Involvement**

The applicant has consulted the local community on the proposals prior to the submission of the planning application. An information leaflet providing an explanation of the proposed development, and welcoming any questions / comments, was issued to all neighbouring properties on 27th October 2021. A total of 109 properties were consulted. A copy of the leaflet issued is provided at Appendix 1 of the application Statement.

A total of 6 residents submitted comments regarding the proposals. Of the comments received 3 residents were supportive of the proposals, stating that the development was a good use of space and would improve the area. 1 comment was neutral and asked questions focusing on the development of the wider Paper Mill site and the associated road improvements.

2 residents did not support the proposals citing concerns over parking provision, open space and the height of the proposed new homes. These items have been considered in detail as part of the design evolution of the scheme and are discussed in the Design and Access Statement.

Following the letter drop a face to face consultation session was held on Tuesday 7th December and which was attended by 5 households and a Ward Councillor. Queries raised at the event focused on parking provision, access during construction, timescales for the development and road improvement works. The project team was able to positively respond to the queries and explain the evolution in the design of the scheme. The majority of residents confirmed that they were supportive of the principle of the development.

### **Layout**

The development has maintained appropriate aspects distances to reflect the terraced grain of the area and not appear over dominant in relation to parking. This type of approach is set out as reasonable within SPD6. The apartment parking would be broadly centrally located within the site and the housing parking provided towards the rear of the properties within curtilage.

The intervening apartment and visitor spaces would be landscaped and use differential materials to separate out the parking areas from the highways.

Generous rear gardens would be provided for the dwellings driven mainly by a water mains infrastructure pipe through parts of the gardens.

An area of amenity space would be provided to the north of the apartment block and beyond would be an area of open space.

Overall the development would not exceed the density and grain of the area and would provide a respectful infill development as part of the first phase of the ELPM redevelopment site. As such the development is considered to comply with EN1/2, H2/1 and H2/2 of the UDP and design guidance of Chapter 12 of the NPPF.

### **Design and Appearance**

The proposed overall design features of the scheme have been influenced by the industrial heritage of the former paper mill and the proposed scheme would create a link in terms of external appearance and design which is centred upon a modern interpretation of industrial architecture.

The apartments would be 3 storey in height with strong pitched roof lines and vertical fenestration patterns. The proposed terraces would also be 3 storey in height to incorporate room in the roofspace. Design references have been taken from the previous mill buildings and the surrounding housing stock and facades and window rhythms would be regular and design features within the apartments and residential units. There would be some contemporary elements within the dwellings but overall, the facades would be appropriate for the new development.

Both the apartments and terraced houses would incorporate red brick facades and grey roof tiles to maintain consistency throughout the development and design features include projected feature brick detailing, 'dummy' brick chimney features and pressed metal parapet capping.

Within the context of the area and with references to the historical elements of the site it is considered the proposed design and appearance would be acceptable and would comply with EN1/2, H2/1 and H2/2 of the UDP and design guidance of Chapter 12 of the NPPF.

**Impact on residential amenity** - SPD6 provided guidance on separation distances between residential developments and generally 20m between habitable room windows and 13m between blank walls and a habitable room window are considered acceptable. Differences in levels and additional storeys tend to require an additional 3m separation.

In terms of the relationship of the proposed row of terraced houses, separation to the houses opposite on Baybutt Street would be slightly below general standards. However, with the intervening road, front gardens and within the patterns of existing urban form, this relationship would not be unusual and has been accepted for other similar development types. This would be similarly so for the apartment block which would face the blank gable walls of the terraces which are side-on to Cunliffe Street and as such privacy to occupiers of these houses would not be significantly affected.

The proposed apartment block would only face one primary elevation, No 6 Pollitt Street, at which point the apartment block windows opposite are 14.9m away. The 3 windows immediately opposite 6 Pollitt Street could be obscure glazed to minimise any overlooking impact, if that was considered beneficial. That said, the general outlook from No 6 would be bettered by the overall improvements and landscaping which form part of the development proposals.

In consideration of the above, the proposed development would reflect the urban grain and density patterns which characterise the surrounding area and as such, it is considered that on balance, with the benefit of improvements to open spaces, the proposed development would not have a significantly detrimental impact on the privacy or outlook of the adjacent houses.

The proposed development is therefore considered to comply with policies H2/1, H2/2, EN1/2 and SPD6.

## **Ecology**

### Summary

The developer's ecological consultant identified no significant ecological issues. Issues relating to bats, nesting birds, invasive species and impacts on the watercourse can be resolved via condition. GMEU also recommend a condition be applied to ensure appropriate biodiversity net gain is provided for the development.

### Bats

The buildings and trees on the site were visually surveyed for bats and two trees subject to two further emergence surveys. No evidence of roosting bats was found with no activity identified until well after dusk. GMEU are satisfied that it is very unlikely that the conservation status of bats will be impacted upon by the development. The building which had negligible potential is also being retained. Reasonable avoidance measures have been recommended for the felling of the two trees with moderate bat roosting potential. GMEU recommend a condition that the removal of trees be carried out in accordance with the details contained section 5.4.9 and 5.4.10 of the ecological assessment as already submitted with the planning application.

### Nesting Birds

Trees, shrubs, scrub and ivy will be lost as a result of the current proposal potential bird nesting habitat. All British birds nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife & Countryside Act 1981, as amended. GMEU recommend a condition to restrict the removal of trees and vegetation.

### Invasive species

Japanese knotweed and wall cotoneaster were recorded on the site. Both are included within schedule 9 part 2 of the Wildlife & Countryside Act 1981, as amended. It is an offence to introduce or cause to grow wild any plant listed under this schedule. The Japanese knotweed was recorded as already undergoing treatment. Wall cotoneaster is relatively easy to control being primarily an issue on calcareous land and would not be regarded as growing in the wild at this location. GMEU recommend however a condition for the submission of a method statement or eradication /control of invasive species.

### Culverted Watercourse

A stream is culverted under the site. There are risks of negative impacts on the water quality and flow rates of the Brook during and post construction if there are any surface water connections directly in to the Brook.

GMEU recommend that any such drainage is identified prior to any earthworks and capped and that any new surface water drainage directly in to the Brook subject to measures to ensure no increase in flow to the Brook, with suitable interceptors to prevent increased levels of sediment and pollution entering the watercourse. The details can be conditioned.

### Contributing to and Enhancing the Natural Environment

Section 174 of the NPPF 2021 states that the planning policies and decisions should contribute to and enhance the natural and local environment.

GMEU have commented that the development would result in the loss of around 0.1ha of mixed scrub of a moderate ecological value in what appears to be poor condition, just over 0.2ha of amenity grassland of a low ecological value habitat automatically regarded as poor condition, a number of mature trees and associated bird nesting and bat roosting opportunities.

The applicant has submitted an Assessment of Biodiversity Net Gain Report and Biodiversity Metric. A landscaping scheme has been submitted which includes tree and vegetation planting, native hedge planting, formation and pathways and seating areas.

GMEU comment that the biodiversity net gain report demonstrates that it is technically feasible to achieve a very small net gain on site. The margins are however tight and rely on all habitats created to achieve moderate ecological value, which for such small areas is not guaranteed. It should also be noted though not an objection that it would not comply with the developing net gain legislation as it achieves only a 0.41% net gain.

No detail has also as yet been agreed in terms of the planting schedule to support the net gain assessment, ie suitable species to achieve the conditions that are forecast. Similarly the net gain notes the provision of bird and bat boxes as additional mitigation that should be taken in to account, which GMEU agree under current guideline is acceptable but have however not as yet seen the bat and bird box proposals.

The site presents opportunities to provide areas of open spaces which would be integral to the overall development proposals. It is therefore envisaged that 3 areas of the site would become quality and usable public areas for amenity purposes, which would comprise forming a landscaped gateway to the site between CockClod Street and Pollitt Street, a

small parcel of land fronting Howard Street and a larger undevelopable piece of land to the south which would not only create visual interest and improve public accessibility but would link to the wider ELPM site in the future.

The proposed landscaping plan shows that open amenity areas would also provide permeable links through the site and deliver a betterment in terms of quality of amenity for local residents and future occupiers of the development.

However, given the above assessment by GMEU, they advise a condition be applied to ensure that -

- the final planting schedules are in line with the habitats proposed in the net gain assessment;
- that a landscape and environmental management plan is provided to demonstrate how the conditions forecast will be achieved and maintained and;
- bat and bird box proposals provided.

It is therefore considered that with the inclusion of an appropriately worded condition, the proposed development would be acceptable and comply with the principles of the NPPF and policies H2/2, EN1/2 and EN1/3.

## **Highways**

### Parking provision

For the proposed terrace properties, the scheme would provide 14 spaces in total, two parking spaces for each of the dwellings (16 is the policy maximum), which given the location and SPD would be at the upper policy level and therefore considered appropriate and acceptable for the scale of development.

The apartment scheme would provide 14 spaces. Policy maximum provision would be 21 spaces.

The location of the site is highly sustainable and is within very close proximity to both bus, Metrolink and the town centre. Given these factors, it is considered that it would not be appropriate to seek higher levels of parking than shown and as such the provision is considered acceptable.

The proposed site plan also indicates there would be scope for 2 EV chargepoints which.

It is therefore considered that given the scale of development, its proximity to sustainable methods of transport and the town centre and given the development would provide 100% affordable accommodation, the proposed development is considered to provide adequate levels of parking and would comply with UDP policies and the principles of the NPPF.

## **Flood Risk**

The site is within Flood risk Zones 2 and 3 as identified in the Environment Agency (EA) mapping system.

The EA originally objected to the application as the submitted Flood Risk assessment failed to comply with the requirements for site specific flood risk assessments as set out in paras 30-32 NPPG.

Information has since been provided by the applicant which has addressed the concerns of the EA who have now withdrawn their objection and recommended a condition that residential related structures (such as sheds) be prohibited in the rear gardens of plots 1-7. A condition is also recommended for measures to deal with the management of invasive species which has also been identified in the response by GMEU above.

It is therefore considered that with appropriate conditions, the proposed development would be acceptable and comply with the principles of the NPPF and NPPG.

The applicant would also be advised to contact the EA with regard to any requirements for permits to be obtained for any activities will take place.

**Air quality** - Bury Council has been identified by DEFRA as an area requiring to significantly improve air quality. Due to this requirement, this Section recommends a condition to be placed on any grant of permission for the provision of 1 electric vehicle (EV) chargepoint for each dwelling with dedicated parking and a minimum 20% EV chargepoints for unallocated parking.

### **Response to objections**

The land was not the Council's to 'gift' to the residents of Bury and the Council does not 'gift' land in this way.

The applicant has responded directly to the comments raised about parking provision and its designation.

- car parking to apartments - consideration given amount of provision balanced with prevention of a dominance of hardstanding, maintaining pedestrian routes and creating appealing vistas for existing residents. The layout would ensure that the public footpath would be maintained to allow safe pedestrian access.
- Disabled parking - The ground floor apartments have some accessible facilities but would not be wholly compliant but have been designed as 'age friendly'. More disabled parking could be provided but on balance with the total parking, is considered acceptable.
- Designated parking - Great Places' policy is not to designate parking spaces in parking courts as this allows both residents and visitors to park freely. It prevents any wasted spaces being created by those apartments which do not have cars, and instead maximises usage. There is also the intention to upgrade nearby streets to provide residential parking permit areas.
- Proposals include for the future provision of electric car charging points for all the houses and two of the apartment parking spaces. Due to the capital cost of electric cars it is highly unlikely that any residents of the affordable homes will have electric cars in the foreseeable future, however should more than two apartments require charging points then additional will be added by Great Places.
- Impact on existing residents - In terms of separation, the proposed apartment block only faces one primary elevation, that of 6 Pollitt Street, at which point the apartment block windows opposite are 14.9m away. The 3 windows immediately opposite 6 Pollitt Street could also be obscure glazed to minimise any overlooking impact, if that was the preference. All other windows would overlook the side of existing houses.
- Access onto Spring Lane has been considered, including a widening of Cock Clod Street and an improved junction with Spring Lane, which have already been submitted to Planners for consideration as part of the proposals for the development of the adjacent site. It is expected that Cock Clod Street will be brought up to adoptable standards, although this work is outside of the scope of this application.
- All other issues have been covered in the above report.

### **Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015**

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental



conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraph 38 of the National Planning Policy Framework.

**Recommendation:** Approve with Conditions

### Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.  
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings -  
12216-AEW-XX-XX-DR-A-0511 P02 - Location plan  
12216-AEW-XX-XX-DR-A-0512 P02 - Existing site plan  
12216-AEW-XX-XX-DR-A-0513 P07 - Proposed site plan;  
12216-AEW-XX-XX-DR-A-0514 P04 - Proposed site plan including constraints;  
12216-AEW-XX-XX-DR-A-0515 P03 - Proposed street elevations  
12216-AEW-XX-XX-DR-A-0516 P03 - Proposed site plan - boundary treatments  
12216-AEW-XX-XX-DR-A-0517 P02 - Proposed site plan - Waste management  
12216-AEW-XX-XX-DR-A-0518 P02 - Proposed site plan - amenity provision  
12216-AEW-XX-XX-DR-A-0519 P02 - Proposed ground diagram  
12216-AEW-XX-XX-DR-A-0520 P3 - House type 1 - proposed floor plans and elevations  
12216-AEW-XX-XX-DR-A-0521 P3 - House type 2 - proposed plans and elevations  
12216-AEW-XX-XX-DR-A-0523 P03 - Proposed ground and first floor plans - Apartments  
12216-AEW-XX-XX-DR-A-0524 P03 - Proposed second floor and roof plans - Apartments  
12216-AEW-XX-XX-DR-A-0525 P03 - Proposed apartment elevations  
12216-AEW-XX-XX-DR-A-0530 P01 - Proposed site section

Foul & Surface Water Drainage Design Drawing 21-B-12866-200 Rev A - Dated 10/11/21; Proposed Levels and External Works Drawing reference: 21-B-12866-101 Rev C dated 21/12/21;

Visibility plan - 3606-F01

Phase 1 and 2 Geoenvironmental Desk Study and ground stability risk assessment Project No NX418

Tree survey Report December 2021 by tba

Bat activity surveys December 2021 by ERAP

Bat survey report TEP ref 8746.001

Arboricultural Impact Assessment and method statement December 2021 by tba

Landscape proposal and planting plan 6674.03

Tree survey and root protection area 6674.04

Tree protection plan 6674.05

Ecological survey and assessment by ERAP

Assessment of biodiversity net gain April 2022 by ERAP

ERAP Biodiversity Metric 3

FRA (20-B-12866/ELPM/revA dated March 2022 and appendices

Affordable Housing Statement November 2021

and the development shall not be carried out except in accordance with the

drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. No development shall commence unless and until:-

- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

5. Prior to occupation the applicant shall provide 1 electric vehicle (EV) chargepoint (minimum 7kW\*) for each dwelling with dedicated parking and a minimum 20% EV chargepoints (minimum 7kW\*) for unallocated parking.

\*Mode 3, 7kW (32A) single phase, or 22kW (32A) three phase, and for 50kW Mode 4 rapid charging may be required. British Standard BS EN 61851-1:2011 to be used (note this version is due to be replaced by BS EN 61851-1:2019 on 5 July 2022).

Reason. This is to secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to the National Planning Policy Framework.

6. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 or any order revoking and re-enacting that order with or without modification, no structure shall be erected within the rear gardens of plots 1-7 Baybutt Street as delineated in appendix J of the FRA (20-B-12866/ELPM/revA dated March 2022).

Reason. To ensure the erection of structures within the flood zone do not affect flood storage / flow routes and increase flood risk elsewhere, pursuant to Bury Unitary Development Plan Policies EN5/1 - New Development and Flood Risk and Chapter 14 - Meeting the challenge of climate change, flooding and coastal change.

7. The drainage for the development hereby approved, shall be carried out in accordance with principles set out in the submitted Foul & Surface Water Drainage Design Drawing 21-B-12866-200 Rev A - Dated 10/11/21 and Proposed Levels and External Works Drawing reference: 21-B-12866-101 Rev C dated 21/12/21 which was prepared by Carley Daines and Partners Limited. For the avoidance of doubt surface water must drain at the restricted rate of 5 l/s. No surface water will be permitted to drain directly or indirectly into the public sewer. Prior to occupation of the proposed development, the drainage schemes shall be completed in accordance with the approved details and retained thereafter for the lifetime of the development.  
Reason. To promote sustainable development, to ensure a satisfactory form of development and to prevent an undue increase in surface water run-off and to reduce the risk of flooding pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
8. No construction shall commence until details of the means of ensuring the water mains laid within the site boundary are protected from damage as a result of the development have been submitted to and approved by the Local Planning Authority in writing. The details shall outline the potential impacts on the water mains from construction activities and the impacts post completion of the development on the water main infrastructure that crosses the site and identify mitigation measures to protect and prevent any damage to the water main both during construction and post completion of the development. Any mitigation measures shall be implemented in full in accordance with the approved details.  
Reason. In the interest of public health and to ensure protection of the public water supply. To promote sustainable development pursuant to Unitary Development Plan Policies EN5/1- New Development and Flood Risk , EN7/3 - Water Pollution and EN7/5 - Waste Water Management and chapter 14 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
9. Details/Samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.  
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Townscape and Built Design and H2/1 - The Form of New Residential Development.
10. The removal of trees called T1 & T4 have the potential to cause harm to bats as identified in the Ecological Survey and Assessment ERAP ref. 2021-283b and the development hereby approved shall be carried out in accordance with the details contained in section 5.4.9 and 5.4.10 of the Ecological Survey and Assessment.  
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
11. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 - Conservation of the Natural Environment and EN6/3 - Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

12. Prior to any earthworks a method statement detailing eradication and/or control and/or avoidance measures for Japanese knotweed and wall cotoneaster should be supplied to and agreed in writing to the Local Planning Authority. The agreed method statement shall be adhered to and implemented in full unless otherwise agreed in writing by the Local Planning Authority.

Reason. The scheme does not provide full details of the actual extent of invasive species in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

13. No development, site clearance, earth moving shall take place or material or machinery brought on site until a survey of existing surface water drainage has occurred and if any direct connectivity to the culverted brook identified a method statement to protect the Brook from contamination shall also be supplied to and agreed by the Local Planning Authority. All measure will be implemented and maintained for the duration of the construction period in accordance with the approved details.

Reason. To ensure the safe and satisfactory development of the site pursuant to the principles of chapter 15 - Conserving and enhancing the natural environment of the NPPF.

14. No development shall take place until it can be demonstrated that there will be no negative impacts on the ecological potential of the culverted Brook resulting from the disposal of foul water and surface water post-development submitted to and approved in writing by the Local Planning Authority. The details, as approved, shall be implemented in full in accordance with a timetable which has first been agreed in writing by the Local Planning Authority.

Reason. To ensure the safe and satisfactory development of the site pursuant to the principles of chapter 15 - Conserving and enhancing the natural environment of the NPPF and UDP Policy EN6/3 - Features of Ecological Value.

15. Further to the submitted landscaping scheme, details shall also be provided to ensure that -

- the final planting schedules are in line with the habitats proposed in the net gain assessment;
- that a landscape and environmental management plan is provided to demonstrate how the conditions forecast will be achieved and maintained and;
- bat and bird box proposals provided.

The approved details and the approved landscaping scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree planting season,; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving

and enhancing the natural environment of the NPPF.

16. The development hereby approved shall only be developed by or on behalf of the applicant as units to be sold as shared ownership units or as general needs rent affordable housing. The affordable housing provision shall be provided in accordance with the approved Affordable Housing Statement, dated November 2021.

Reason. The proposed development has been granted given the particular circumstances of the applicant following a funding package which provides an opportunity to promote increased affordable housing and to ensure that the development would contribute to satisfying the need for affordable housing provision pursuant to Bury Unitary Development Plan Policy H4/1 - Affordable Housing and the associated Development Control Policy Guidance Note 5 - Affordable Housing Provision In New Residential Developments.

17. Notwithstanding the details indicated on the approved plans, no development shall commence unless and until full details of the following have been submitted on a topographical survey of the site and adjacent adopted highways to, and agreed with, the Local Planning Authority:

- Reconstruction of all affected footways/provision of 2.0m (minimum) new footways abutting the site on Cross Lane, Church Street East, Rectory Lane, Pollitt Street and Howard Street to a scope and specification to be agreed, incorporating the removal of the affected boundary fencing and landscaping features at the junction of Cock Clod Street with Cross Lane, demarcation of the limits of the existing/future adopted highway with flush (at all pedestrian/vehicular access points)/50mm upstand edgings (adjacent to all landscaped amenity areas) and all associated highway and highway drainage remedial works required as a result of the development;
- Provision of adoptable parallel parking lay-by arrangements on Pollitt Street to dimensions, and with entry/exit kerblines, to be agreed and all associated highway and highway drainage remedial works required as a result of the development;
- Revised arrangements at the junction of the un-named back street with Rectory Lane to form a 90 degree junction adjacent to the rear of Plot 7, and all associated highway and highway drainage remedial works required as a result of the development;
- Revised arrangements at the junction of the apartment car park access onto Howard Street with tracked kerb radii, measures to prevent vehicle encroachment from the adjacent disabled parking spaces, measures to prevent the discharge of surface water onto the adopted highway, demarcation of the limits of the adopted highway and all associated highway and highway drainage remedial works required as a result of the development;
- Provision of level pedestrian accesses at the interface with the existing adopted highway, including at either end of the unadopted pedestrian link between Cross Lane and Pollitt Street;
- If deemed redundant by United Utilities as part of the potential Public Open Space proposals at Rectory Lane/Church Street East, reinstatement of the existing access onto the adopted highway;
- Provision of a street lighting assessment of all adopted and unadopted highway used to serve the site and, if required, subsequent scheme of improvements to existing street lighting on the adopted/unadopted highways, incorporating the replacement of all affected street lighting columns as a result of the assessment and proposed access arrangements, in positions to be

agreed.

The details subsequently approved shall be implemented to an agreed programme with all highway works completed prior to the development hereby approved being occupied/brought into use.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/1 - The Form of New residential development, H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict. and HT4 - New Development

18. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
- Photographic dilapidation survey of the footways and carriageways leading to and abutting the site in the event that subsequent remedial works are required following construction of the development, works to the site boundaries and as a result of statutory undertakers connections to the site;
  - Access route for all vehicles to the site from the Key Route Network;
  - Access point(s) for construction traffic from the adopted/unadopted highway and all temporary works required to facilitate access for ground works/construction vehicles;
  - If proposed, details of site hoarding/inward opening gate positions;
  - The provision, where necessary, of temporary pedestrian facilities/protection measures on the adopted/unadopted highway;
  - A scheme of appropriate warning/construction traffic speed signage in the vicinity of the site and its access(es);
  - Confirmation of hours of operation and number of vehicle movements;
  - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site and/or measures to control/manage delivery vehicle manoeuvres;
  - Parking on site or on land within the applicant's control of operatives' and construction vehicles together with storage on site of construction materials;
  - Measures to ensure that all mud and other loose materials are not spread onto the adjacent adopted highways as a result of the groundworks operations or carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials. All highway remedial works identified as a result of the dilapidation survey shall be implemented prior to the development hereby approved being brought into use.

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

20. The visibility splays indicated on approved plan reference 3606-F01 shall be implemented to the written satisfaction of the Local Planning Authority before the development is brought into use and subsequently maintained free of obstruction above the height of 0.6m.  
Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.
21. No development shall commence unless and until details of the proposed arrangements for future management and maintenance of the proposed estate road and street lighting within the development have been submitted to and approved by the local planning authority. The estate road shall thereafter be maintained in accordance with the approved management and maintenance details until such time as a private management and maintenance company has been established.  
Reason. To ensure that the unadopted estate road serving the development is maintained to an acceptable standard in the interests of residential/highway safety; to ensure a satisfactory appearance to the highways infrastructure serving the development; and to safeguard the visual amenities of the locality and users of the highway pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict. and HT4 - New Development
22. The vehicular/pedestrian access/bin collection arrangements and turning facilities within the curtilage of the site, including improvements to the unadopted sections of Cunliffe Street, Baybutt Street and un-named back street to the rear of No.'s 3 - 13 Rectory Lane, indicated on the approved plans shall be provided before the development is first occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.  
Reason. In the interests of residential/highway safety and to minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/1 - The Form of New residential development, H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict. and HT4 - New Development.
23. The car parking indicated on the approved plans, incorporating minimum in-curtilage parking hardstanding lengths of 5.0m and measures to prevent the discharge of surface water onto the adjacent highways, shall be surfaced, demarcated (as appropriate) and made available for use prior to the development hereby approved being occupied and thereafter maintained at all times.  
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict.
23. Bin storage and refuse collection arrangements shall be provided within the curtilage of each dwelling/the site in accordance with Waste Management's 'Guide to Refuse Collection Requirements & Storage Methods for New Developments'.  
Reason. To ensure that adequate bin storage arrangements are provided within the curtilage of each dwelling pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development and EN1/2 - Townscape and

Built Design.

For further information on the application please contact **Dave Marno** on **0161 253 5291**



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Photo 1



Photo 2





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Photo 3



Photo 4





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Photo 5



Photo 6





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Photo 7



Photo 8





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Photo 9



Photo 10





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Photo 11



Photo 12





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Aerial view



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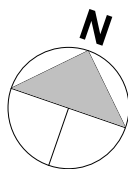


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12216-AEW-XX-XX-M3-A-0001

Client notified of duties: **Jan 2021**  
Principal Designer: **CPC**

Unless noted below, all known hazards have been highlighted on the drawing:




Page 85

Status	Purpose of Issue
S2	Planning

### Location Plan

date	25/11/21	drawn	DS
scale @ A4	1 : 1250	checked	CS



NOTES

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12216-AEW-XX-XX-M3-A-0001

CDM 2015

Client notified of duties: Jan 2021

Principal Designer: CPC  
Notified below, all known hazards have been highlighted on the drawing:

Accommodation Schedule

- 17 1 Bed Apartments (45.0-48.0m<sup>2</sup>)
- 3 2 Bed Apartments (57.15m<sup>2</sup>)

20 Apartments

House Schedule

- 5 3 Bed House (91.1m<sup>2</sup>) Plots 2 & 8
- 2 4 Bed House (101.0m<sup>2</sup>) Plots 1 & 7

7 Houses

27 Total Units

Key

- Site Boundary
- Culvert
- Culvert Easement Zone
- Water Main
- Public Sewer
- Substation
- Existing trees to be removed

Dotted, future Third Party realigned.

CUNLIFFE STREET & BAYBUTT STREET TO BE OFFERED FOR ADOPTION

P04	27/05/22	Lay by parking amended	DS	BR
P03	04/04/22	Revised Following planner/client feedback	DS	BR
P02	20/12/21	Site Boundary Updated and issued for Planning	DS	BR
P01	20/11/21	Issued For Planning	DS	BR
Rev	Date	Description	Drawn by	Checked by

Status Purpose of Issue  
S2 Planning

drawing stage Developed Design

client

Great Places Housing Group

project

Radcliffe, ELPN Island Site

drawing title  
Proposed Site Plan - Including Constraints

date 25/11/21 drawn DS  
scale @ A1 As indicated checked CS

Rev

P04

12216-AEW-XX-XX-DR-A-0514

Dwg No

aew architects  
0161 214 4370  
www.aewarchitects.com

aew



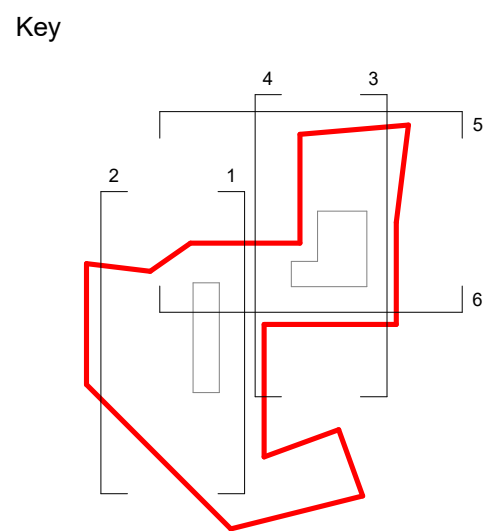
- NOTES
- All dimensions and levels are to be checked on site
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12216-AEW-XX-XX-M3-A-0001

CDM 2015

Client notified of duties: Jan 2021

Principal Designer: GPC

Unless noted below, all known hazards have been highlighted on the drawing.



1 Street Elevation - Baybutt Street  
Scale @ 1 : 100



2 Street Elevation - New Road  
Scale @ 1 : 100



3 Street Elevation - Cock Clod Street  
Scale @ 1 : 100



4 Street Elevation - Pollitt Street  
Scale @ 1 : 100



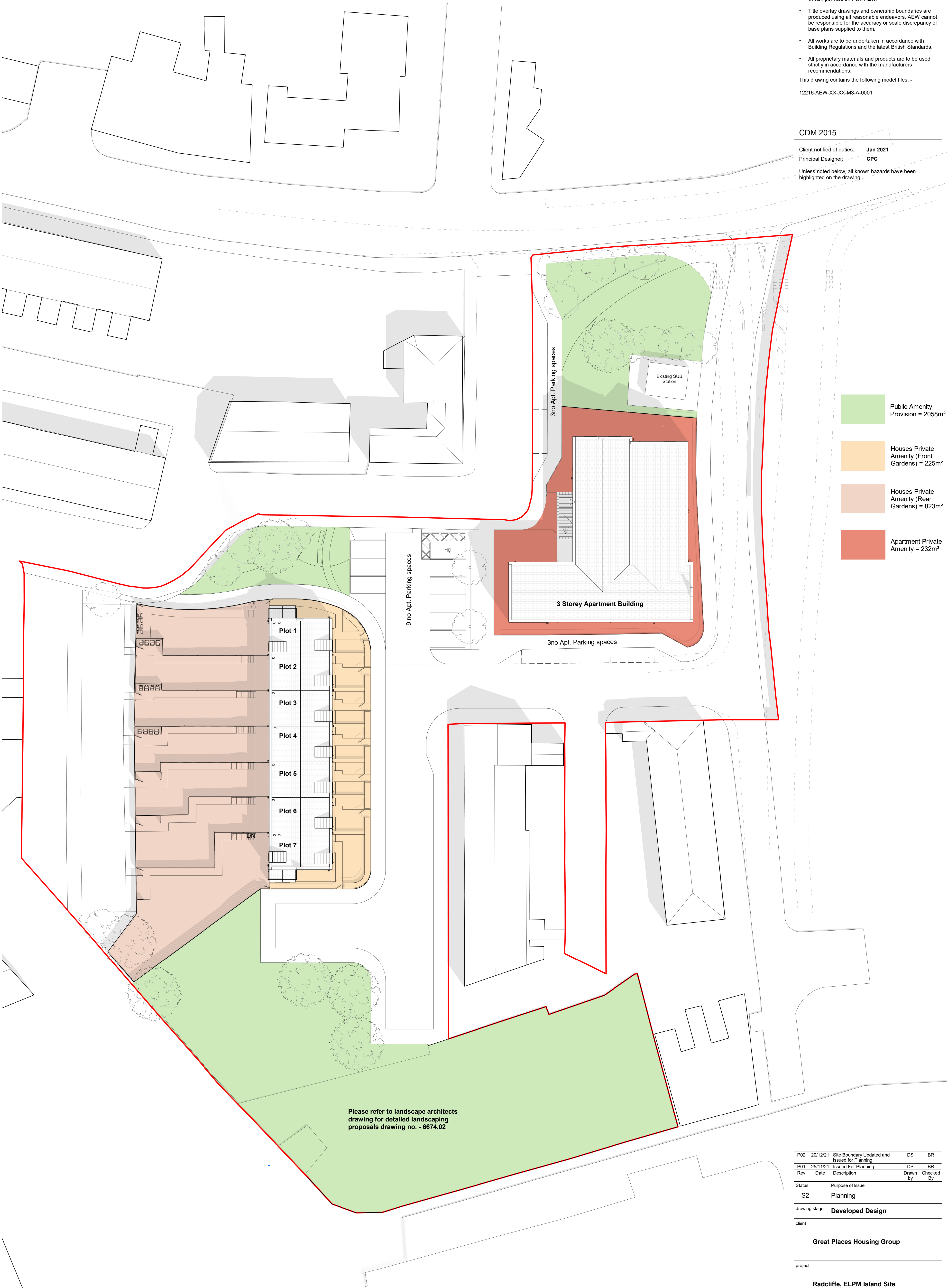
5 Street Elevation - Spring Lane / Cross Lane  
Scale @ 1 : 100



6 Street Elevation - Cunliffe Street  
Scale @ 1 : 100

PO3	30/05/22	Minor alterations to Apartment entrance canopy and southern facade illumination	DS	BR
PO2	21/12/21	Drawing updated following Planning	DS	BR
PO1	25/11/21	Issued For Planning	DS	BR
Rev	Date	Description	Drawn By	Checked By
S2		Purpose of Issue		
		For Information		
drawing stage				
Developed Design				
client				
Great Places Housing Group				
project				
Radcliffe, ELPN Island Site				
drawing title				
Street Elevations - Proposed				
date	18/06/2012	drawn	DAL	
scale @ A0	1 : 100	checked	DS	





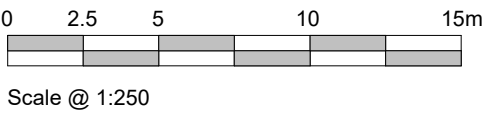
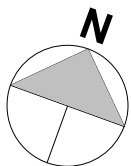
NOTES

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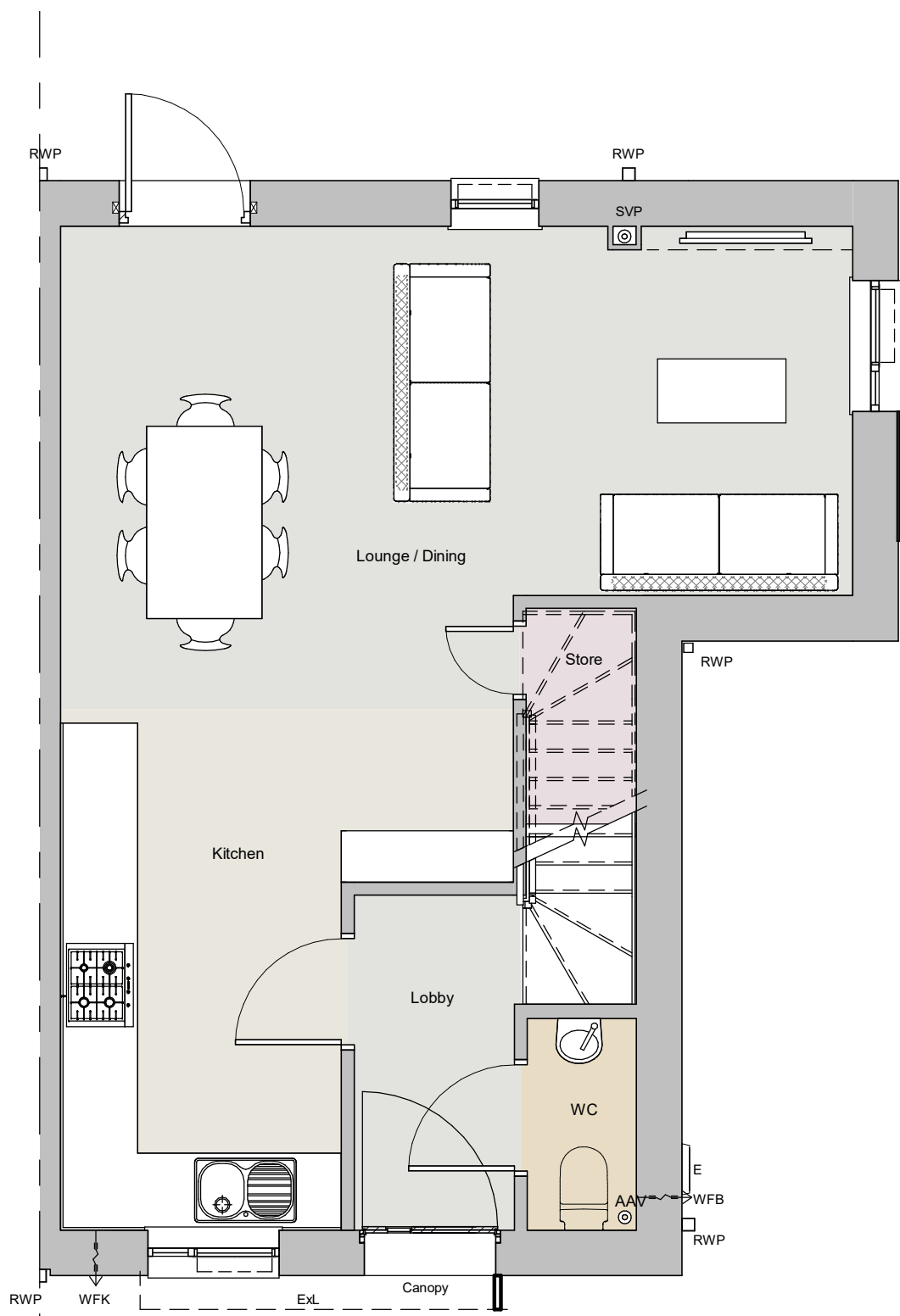
CDM 2015

Client notified of duties: Jan 2021  
Principal Designer: CPC  
Unless noted below, all known hazards have been highlighted on the drawing:

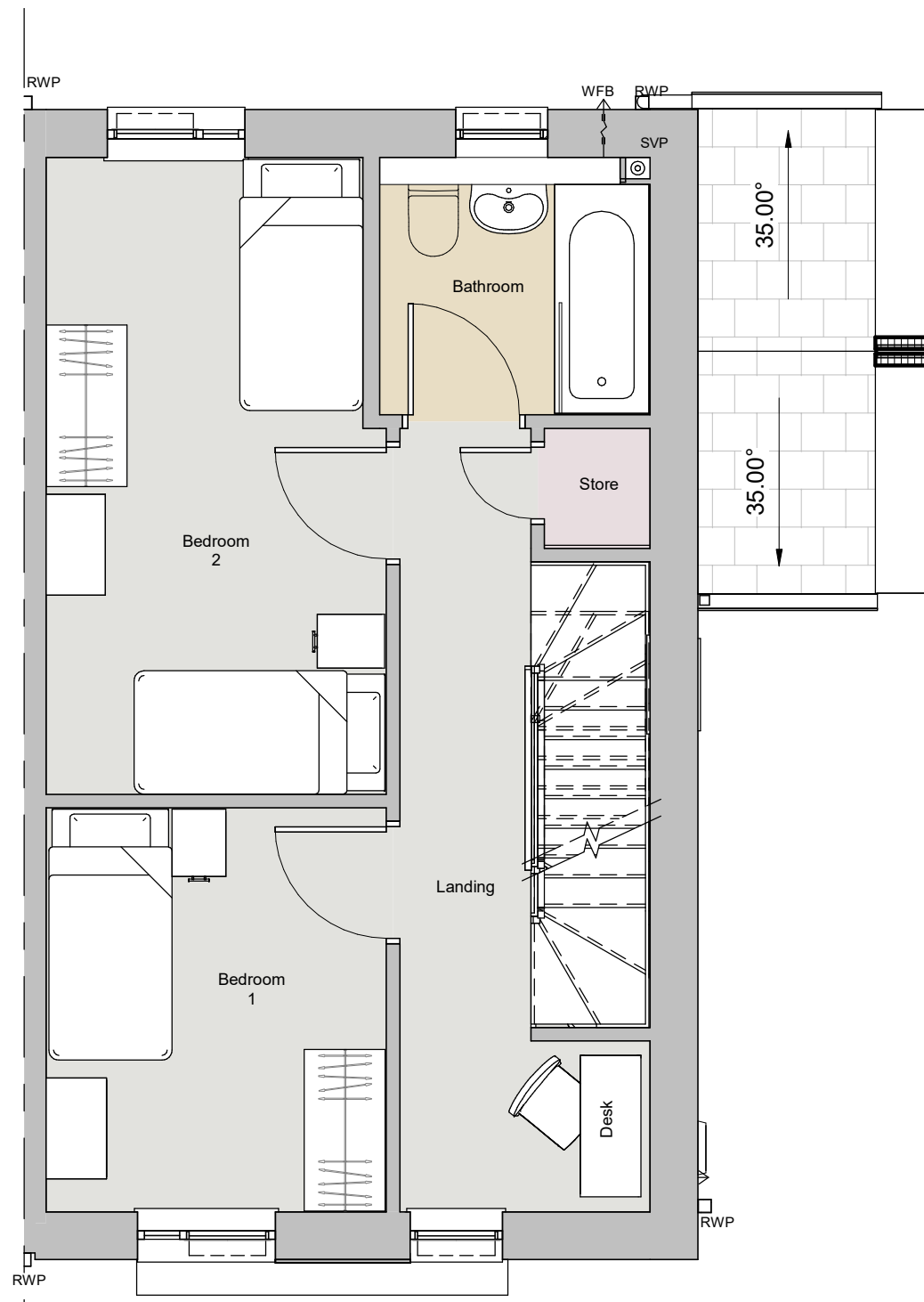
P02	20/12/21	Site Boundary Updated and Issued for Planning	DS	BR
P01	25/11/21	Issued For Planning	DS	BR
Rev	Date	Description	Drawn by	Checked By
Status	Purpose of Issue			
S2	Planning			
drawing stage				
Developed Design				
client				
Great Places Housing Group				
project				
Radcliffe, ELPM Island Site				
drawing title				
Proposed Site Plan - Amenity Provision				
date				
26/11/21	drawn	DS		
scale @ A1	1 : 250	checked	BR	



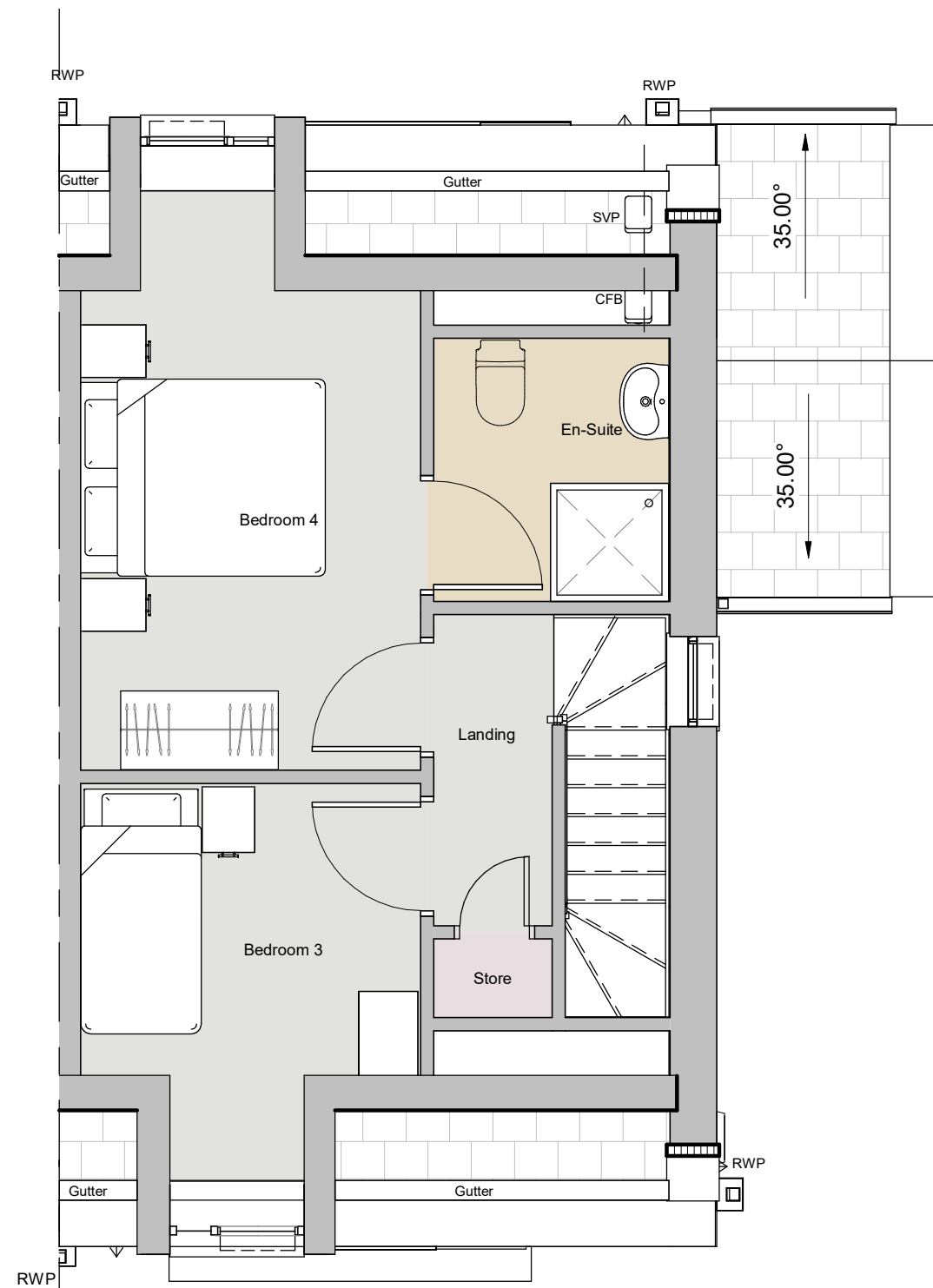




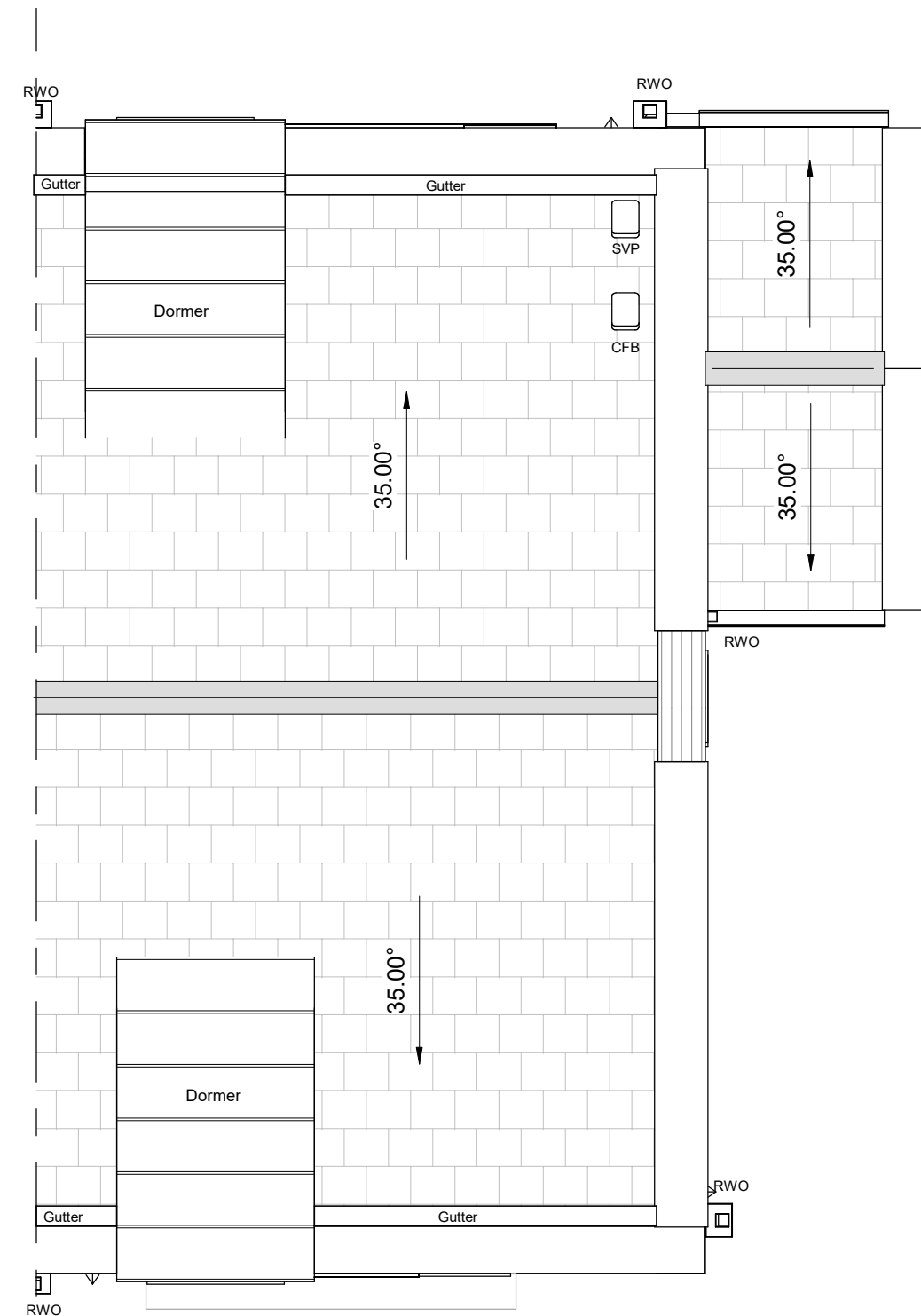
Ground Floor  
Scale @ 1 : 50



First Floor  
Scale @ 1 : 50



Second Floor  
Scale @ 1 : 50



Roof Plan  
Scale @ 1 : 50

#### NOTES

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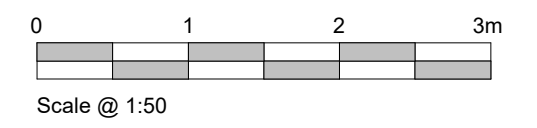
12216-AEW-02-XX-M3-A\_House Type 2 3B5P 84m² [P.01]  
[S0]

#### CDM 2015

Client notified of duties: **2020**  
Principal Designer: **CPC**

Unless noted below, all known hazards have been highlighted on the drawing:

House Type 2 - 4B6P - Area Schedule (GIA)	
Name	Area
HT2 - 00 Ground Floor	39.9 m²
HT2 - 01 First Floor	35.1 m²
HT2 - 02 Second Floor	26.1 m²
	101.0 m²

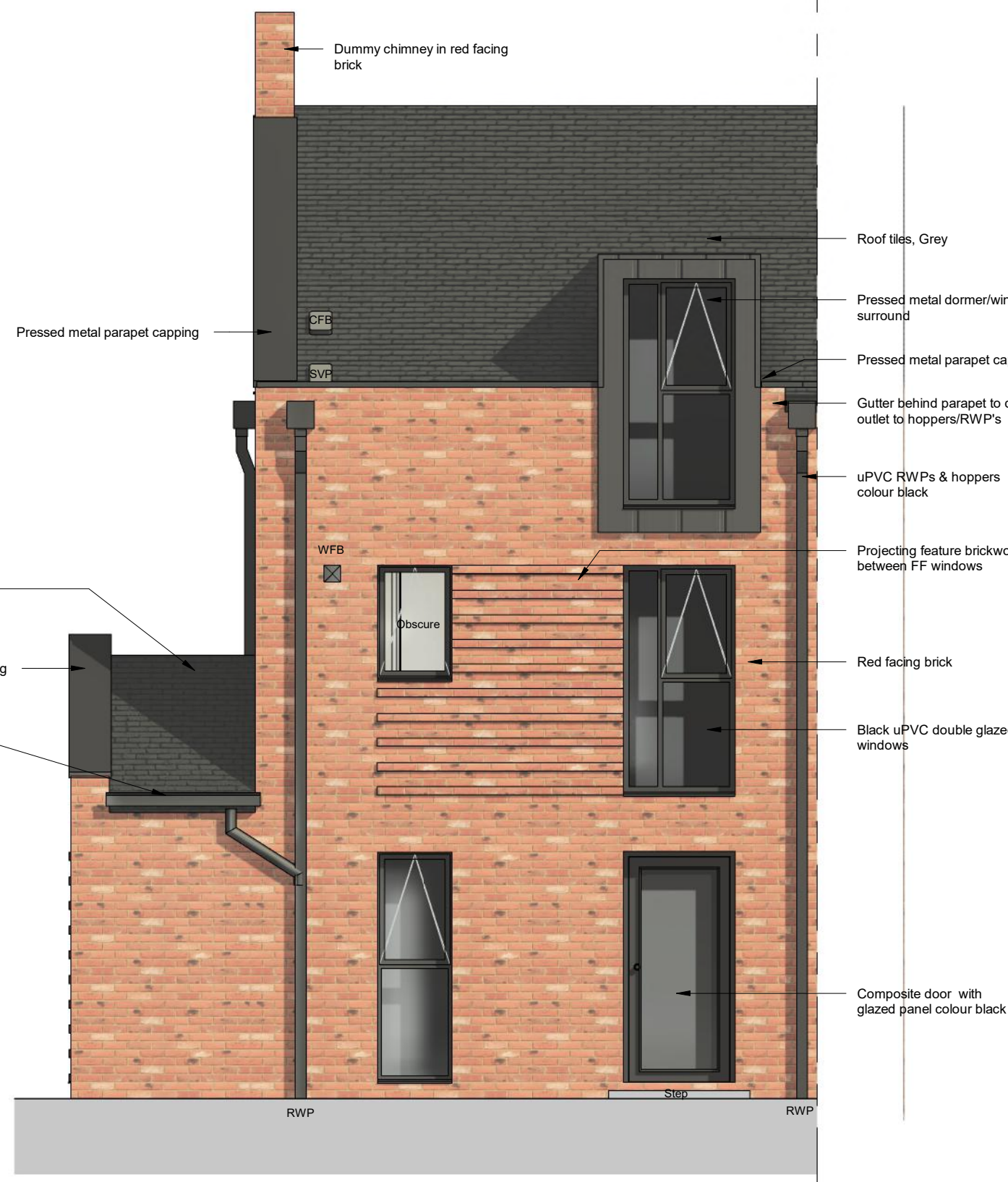


#### KEY

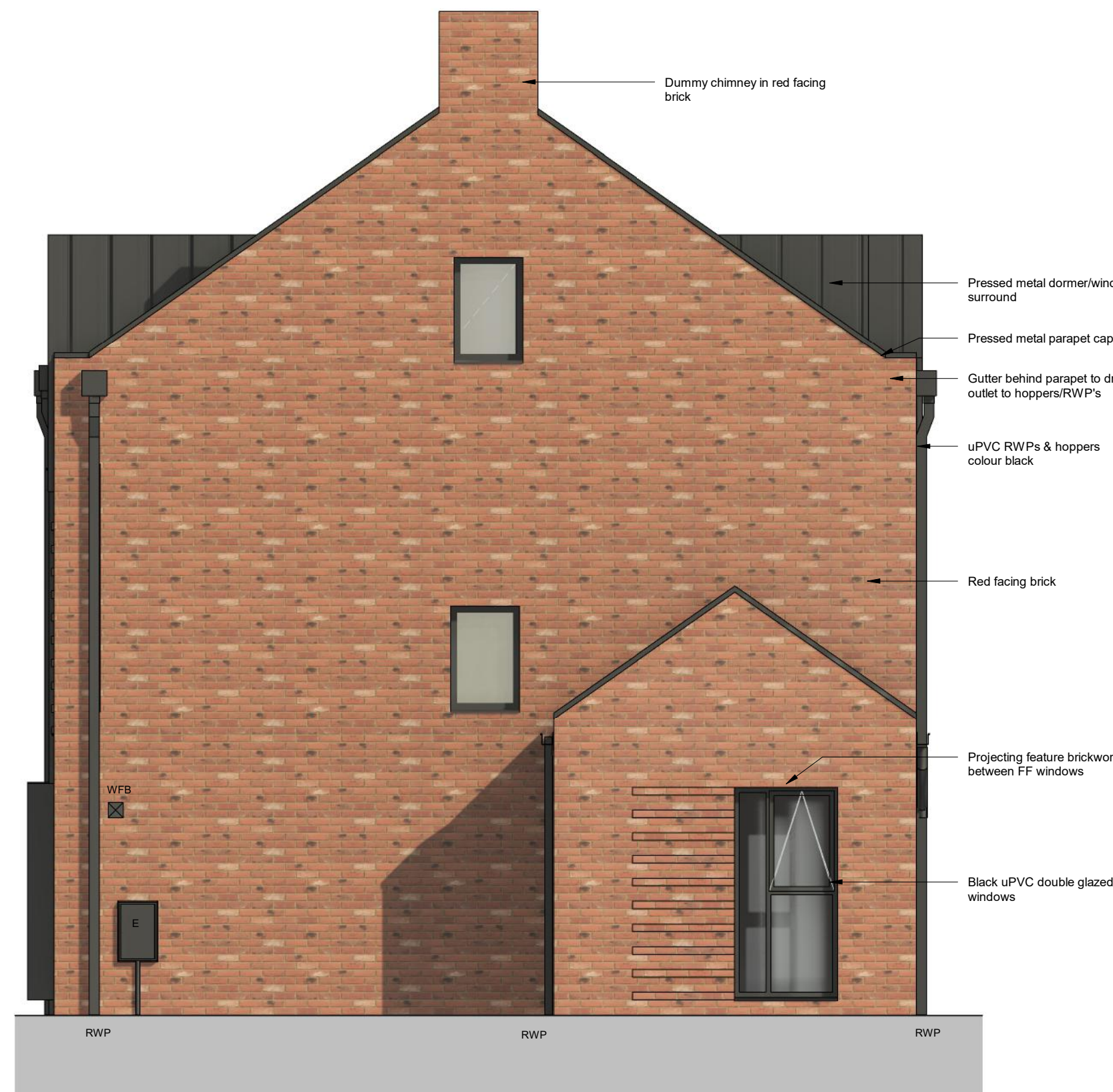
- RWP Rain water downpipe  
RWO Rain water outlet  
SVP Soil Vent pipe  
AAV Air Admittance Valve  
WFB Wall mounted bathroom extract fan (min extract rate of 15 ltr/sec)  
CFB Cavity Fire Barrier  
WFK Wall mounted Kitchen extract fan (min extract rate of 60 ltr/sec)  
BF Boiler flue  
EXL Low Energy External light fitting with PIR sensor (min efficacy of 40 lumens per circuit watt) to comply with SBD.  
L Glazing with laminated glass outer leaf  
T Glazing with toughened inner leaf  
E Egress windows to comply with building Regulations requirements  
OB Glazing with Obscure glass  
G Gas Meter  
E Electricity Meter  
RL Roof Light  
AH Access Hatch



Front Elevation  
Scale @ 1 : 50



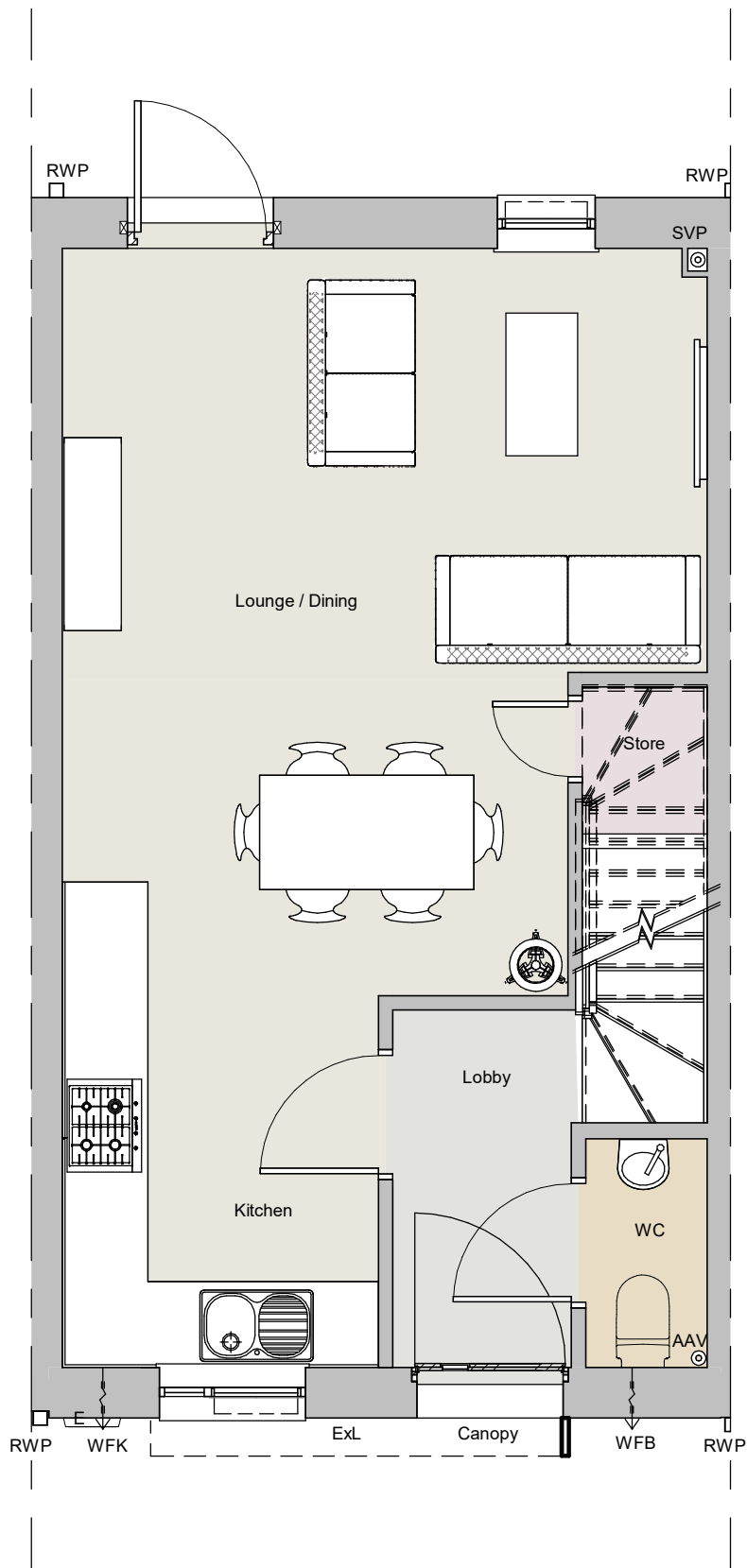
Rear Elevation  
Scale @ 1 : 50



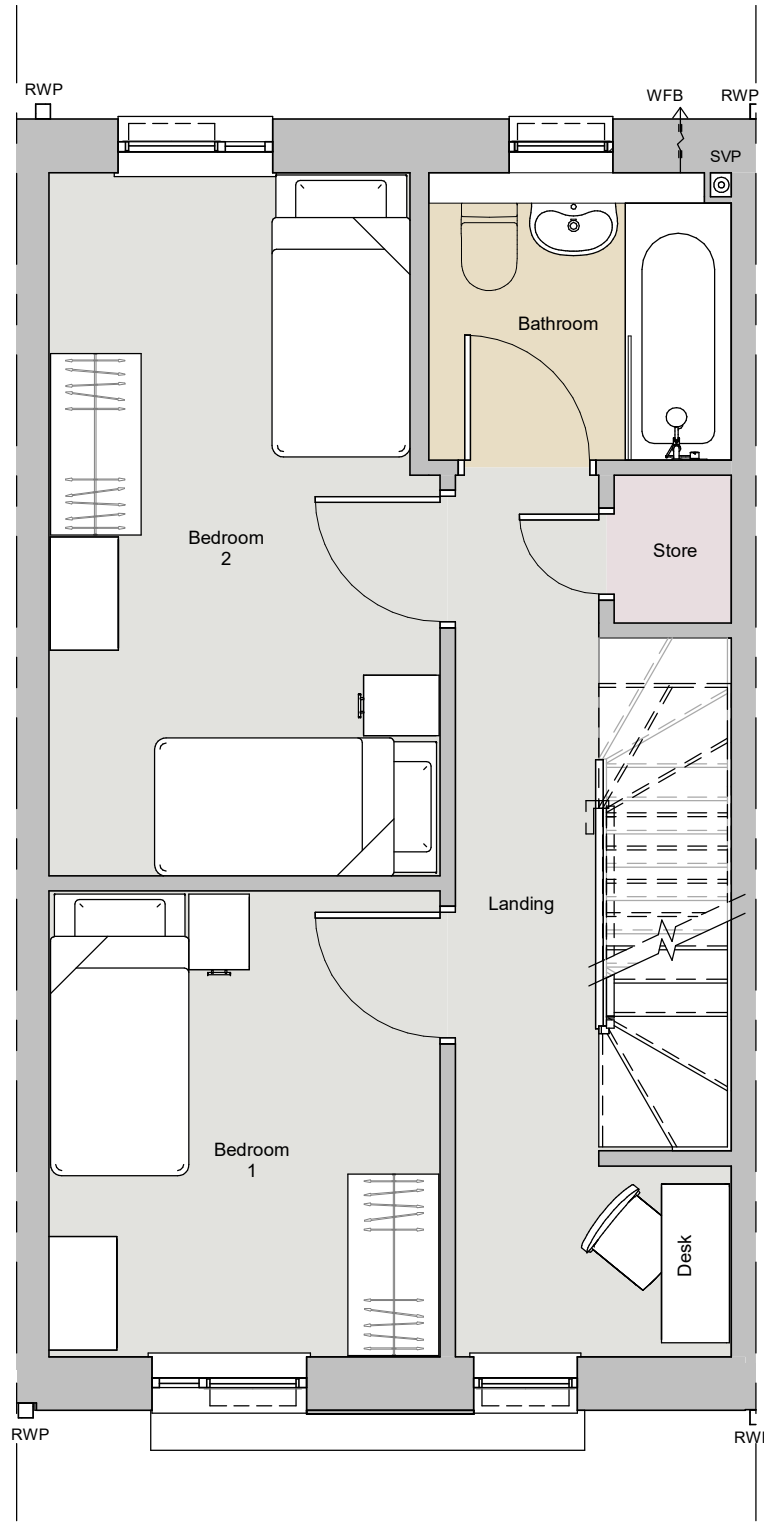
Gable Elevation  
Scale @ 1 : 50

P3	24.11.21	DS	BR
Planning Submission			
P2	22.10.21	DS	BR
Updated following client feedback			
P1	08.10.21	DAL	DS
Initial Issue			
REV	Date	Drawn by :-	Checked by :-
Status	Purpose of Issue		
S2	Planning		
drawing stage			
Developed Design			
client			
Great Places Housing Group			
project			
Radcliffe - ELPM Island Site			
drawing title			

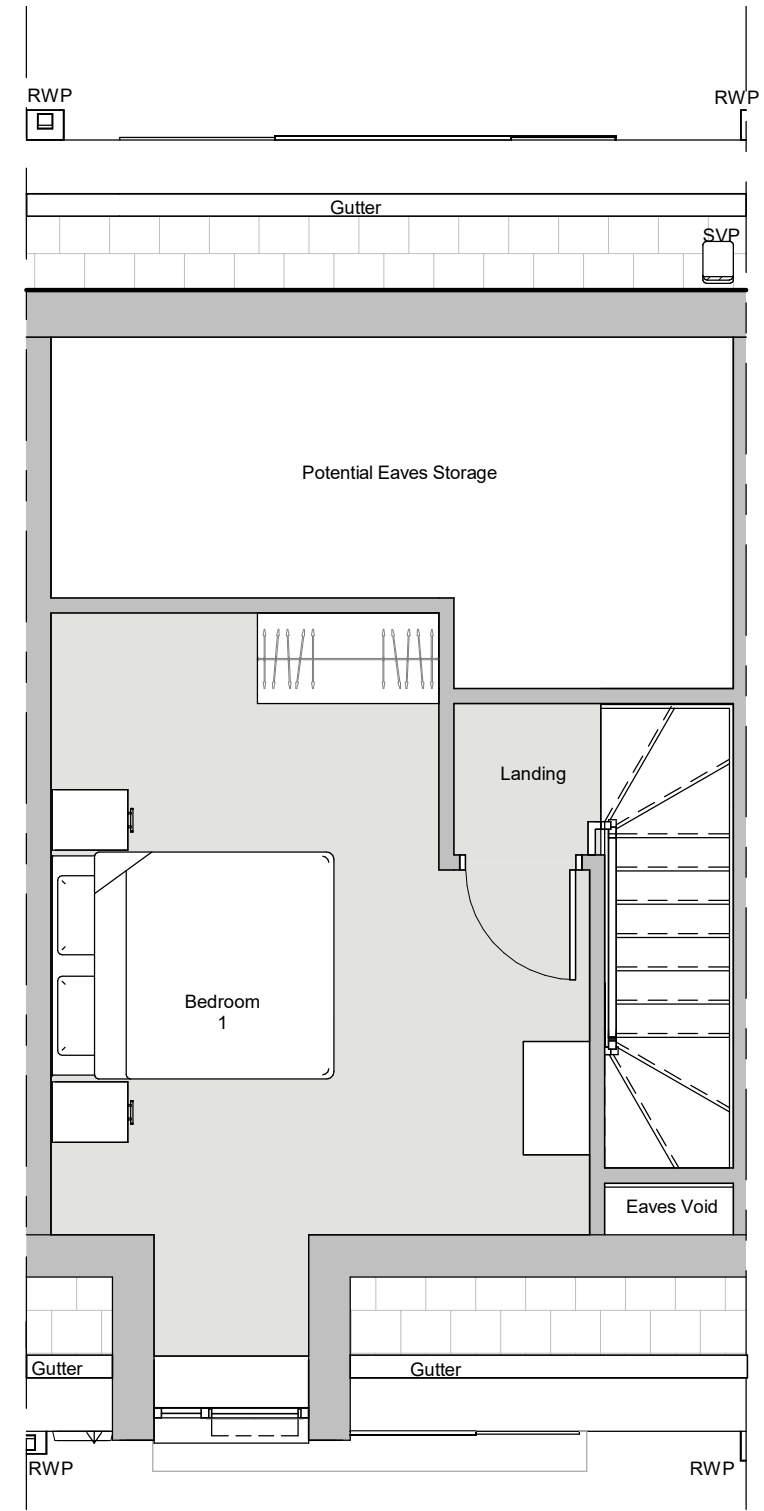




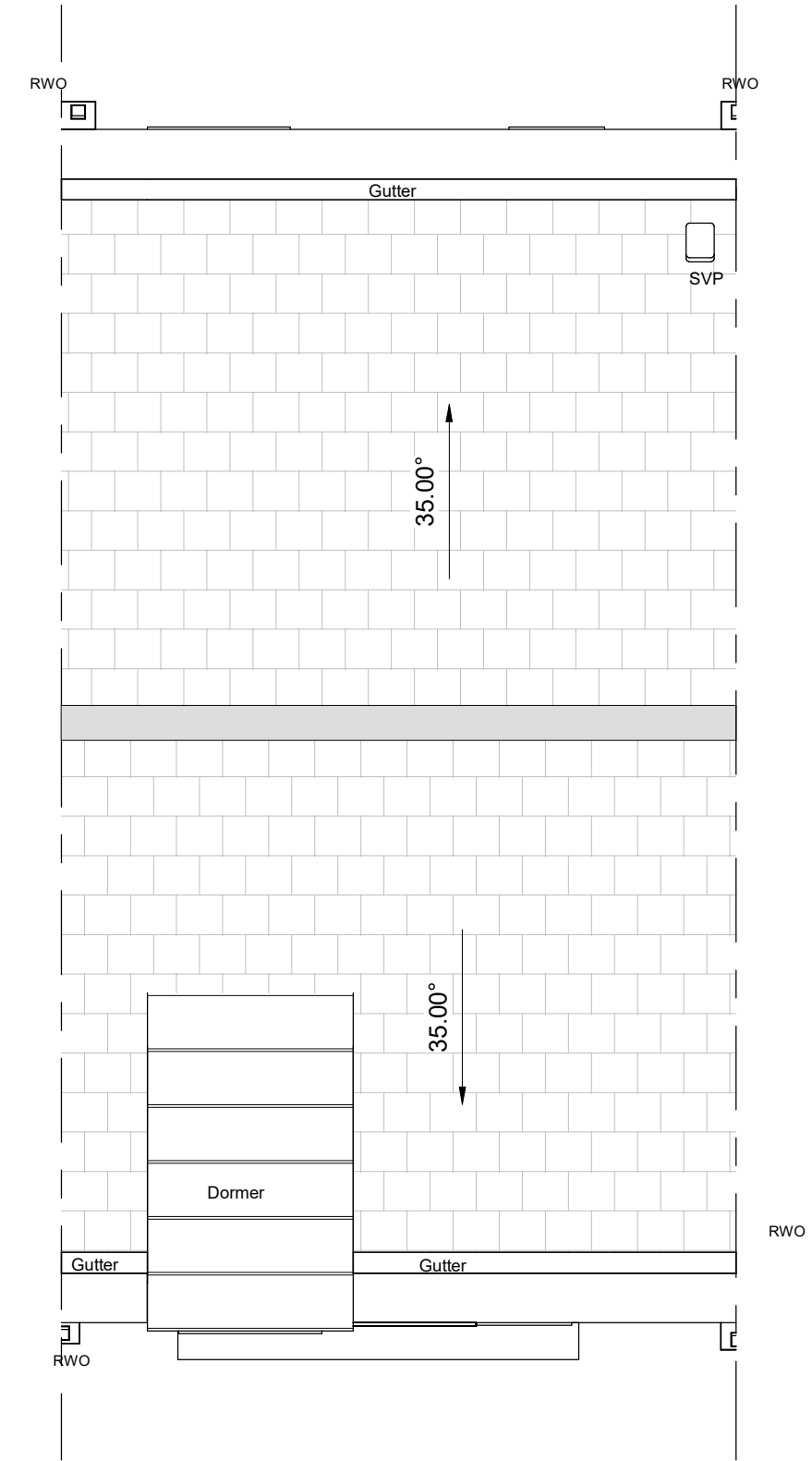
Ground Floor  
Scale @ 1 : 50



First Floor  
Scale @ 1 : 50



Second Floor  
Scale @ 1 : 50



Roof Plan  
Scale @ 1 : 50



Front Elevation  
Scale @ 1 : 50



Rear Elevation  
Scale @ 1 : 50

## NOTES

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This drawing contains the following model files :-

12216-AEW-02-XX-M3-A\_House Type 2 3B5P 84m² [P.01]  
[S0]

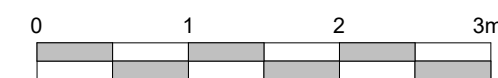
## CDM 2015

Client notified of duties: **2020**  
Principal Designer: **CPC**

Unless noted below, all known hazards have been highlighted on the drawing:

## House Type 1 - 3B5P - Area Schedule (GIA)

Name	Area
HT1 - 00 Ground Floor	35.3 m²
HT1 - 01 First Floor	35.3 m²
HT1 - 02 Second Floor	16.9 m²
	87.5 m²



Scale @ 1:50

## KEY

- RWP Rain water downpipe  
RWO Rain water outlet  
SVP Soil Vent pipe  
AAV Air Admittance Valve  
WFB Wall mounted bathroom extract fan (min extract rate of 15 ltr/sec)  
CFB Cavity Fire Barrier  
WFK Wall mounted Kitchen extract fan (min extract rate of 60 ltr/sec)  
BF Boiler flue  
EXL Low Energy External light fitting with PIR sensor (min efficacy of 40 lumens per circuit watt) to comply with SBD.  
L Glazing with laminated glass outer leaf  
T Glazing with toughened inner leaf  
E Egress windows to comply with building Regulations requirements  
OB Glazing with Obscure glass  
G Gas Meter  
E Electricity Meter  
RL Roof Light  
AH Access Hatch

P3	24.11.21	DS	BR
Planning Submission			
P2	22.10.21	DS	BR
Floor plan revised following client feedback			

P1	08.10.21	DAL	DS
Initial Issue			

REV	Date	Drawn by -	Checked by -
-----	------	------------	--------------

Status	Purpose of Issue
S2	Planning

drawing stage **Developed Design**

client

**Great Places Housing Group**

project

**Radcliffe - ELPM Island Site**

drawing title

**House Type 1 - 3B5P - Plots 2-6**

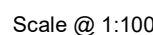
date	25.11.21	drawn	DAL
scale @ A1	1 : 50	checked	DS



This drawing contains the following model files: -

12XXX-AEW-XX-XX-M3-A-0001

Unless noted below, all known hazards have been highlighted on the drawing:



Area Schedule (GIA)		Apartment Schedule	
Level	Area	Type	Count
00 Ground Floor	436.4 m <sup>2</sup>	1B2P	17
01 First Floor	432.6 m <sup>2</sup>	2B3P	3
02 Second Floor	428.9 m <sup>2</sup>	20	
1297.9 m <sup>2</sup>			

Area Schedule (GIA)	
Level	Area
00 Ground Floor	436.4 m <sup>2</sup>
01 First Floor	432.6 m <sup>2</sup>
02 Second Floor	428.9 m <sup>2</sup>
	1297.9 m <sup>2</sup>

Apartment Schedule	
Type	Count
1B2P	17
2B3P	3
20	

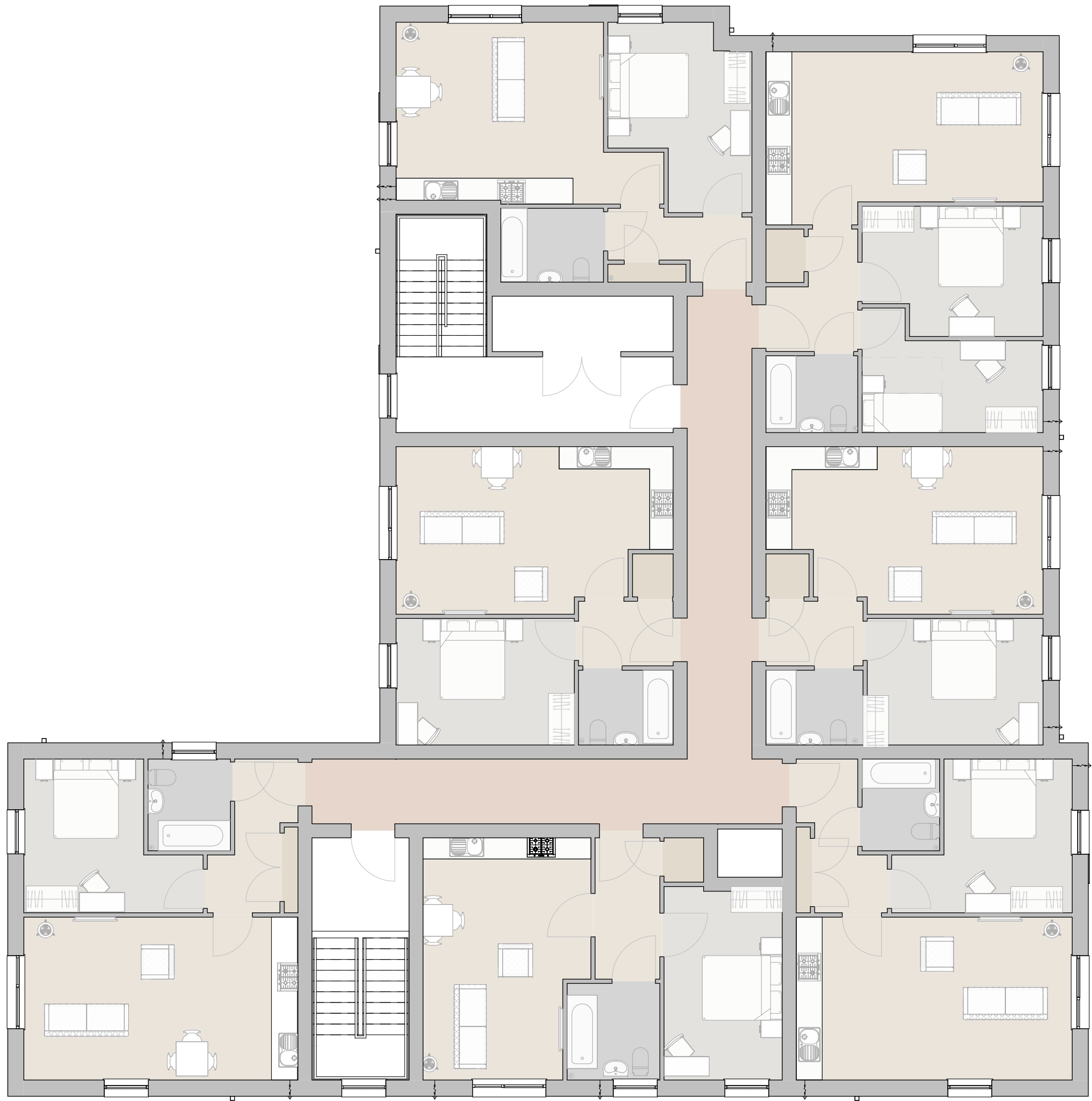
P03	30/05/22	Minor amendment to entrance canopy and window fenestration to south elevation. Alterations to canopy following fire engineer feedback.	DAL	DS
P02	25/11/21	Planning Submission	DS	BR
P01	25/10/21	Issued For Client Feedback & Costing	DS	BR
Rev	Date	Description	Drawn by	Checked By
Status	Purpose of Issue			
S2	For Comment			

**Proposed Ground & First Floor Plans -  
Apartments**

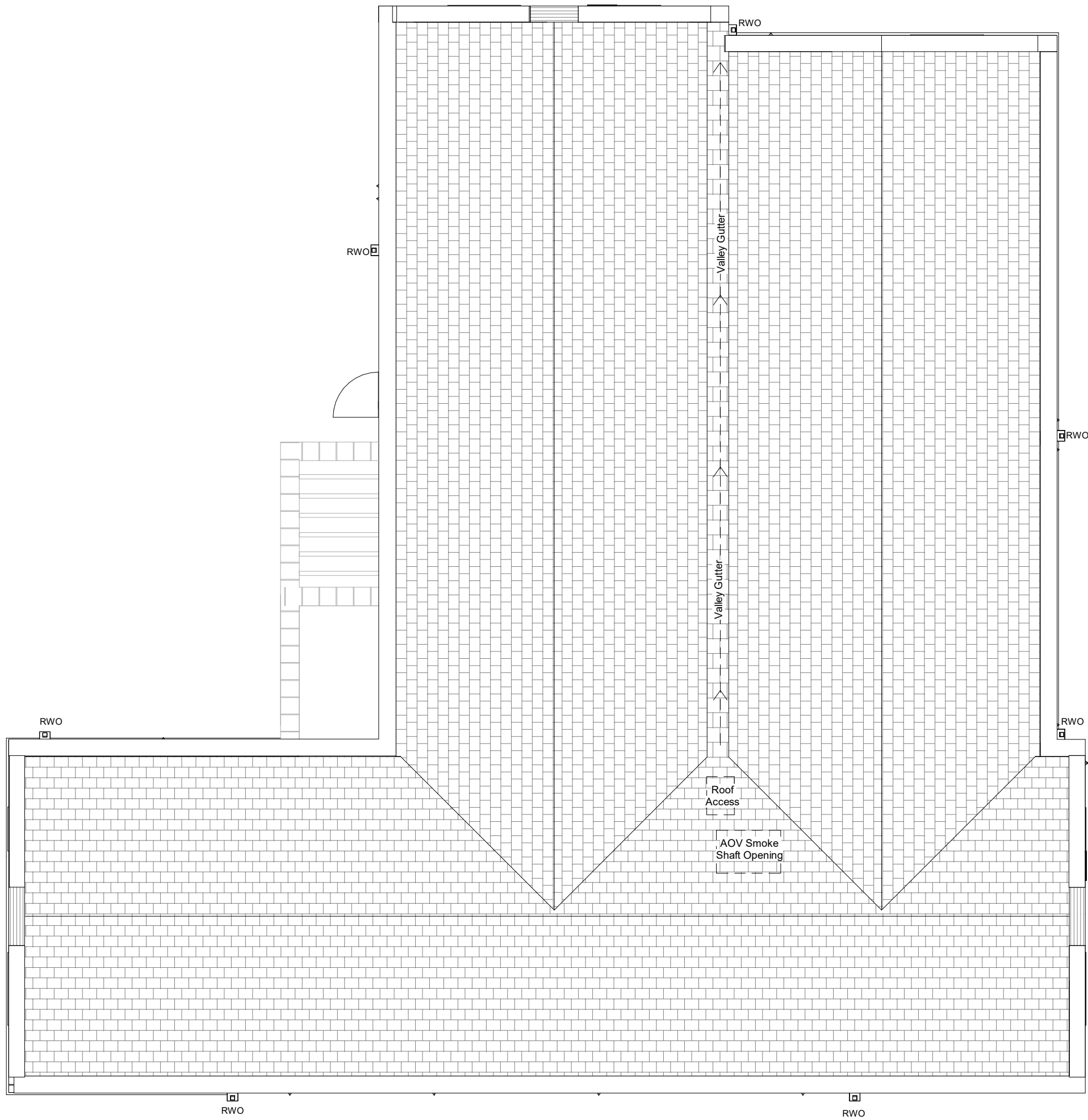
date	22/12/20	drawn	DS
scale @ A1	1 : 100	checked	BF



Refer to Drawing no - 12216-AEW-XX-XX-DR-  
A-524 -Proposed Second Floor & Roof Plan -  
Apartments For Second Floor & Roof Plan Layout



Proposed Second Floor Plan  
Scale @ 1 : 100



Proposed Roof Plan  
Scale @ 1 : 100

Refer to Drawing no - 12216-AEW-XX-XX-DR-A-523-Proposed Ground & First Floor plans - Apartments For Ground & First Floor Layout

NOTES

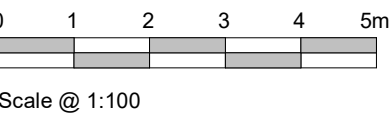
- All dimensions and levels are to be checked on site.
- Any discrepancies are to be reported to the architect before any work commences
- This drawing shall not be scaled to ascertain any dimensions. Work to figured dimensions only.
- This drawing shall not be reproduced without express written permission from AEW.
- Title overlay drawings and ownership boundaries are produced using all reasonable endeavors. AEW cannot be responsible for the accuracy or scale discrepancy of base plans supplied to them.
- All works are to be undertaken in accordance with Building Regulations and the latest British Standards.
- All proprietary materials and products are to be used strictly in accordance with the manufacturers recommendations.

This drawing contains the following model files: -  
12XXX-AEW-XX-XX-M3-A-0001

CDM 2015

Client notified of duties: **December 2020**  
Principal Designer: **CPC**

Unless noted below, all known hazards have been highlighted on the drawing:



Area Schedule (Rentable)			
Name	Level	Area	Type
Apt 1 - GF	00 Ground Floor	47.3 m²	1B2P
Apt 2 - GF	00 Ground Floor	44.9 m²	1B2P
Apt 3 - GF	00 Ground Floor	47.5 m²	1B2P
Apt 4 - GF	00 Ground Floor	44.6 m²	1B2P
Apt 5 - GF	00 Ground Floor	57.2 m²	2B3P
Apt 6 - GF	00 Ground Floor	44.4 m²	1B2P
Apt 7 - FF	01 First Floor	47.7 m²	1B2P
Apt 8 - FF	01 First Floor	47.7 m²	1B2P
Apt 9 - FF	01 First Floor	47.1 m²	1B2P
Apt 10 - FF	01 First Floor	45.6 m²	1B2P
Apt 11 - FF	01 First Floor	57.3 m²	2B3P
Apt 12 - FF	01 First Floor	45.3 m²	1B2P
Apt 13 - FF	01 First Floor	45.8 m²	1B2P
Apt 13 - SF	02 Second Floor	47.7 m²	1B2P
Apt 15 - SF	02 Second Floor	47.7 m²	1B2P
Apt 16 - SF	02 Second Floor	47.1 m²	1B2P
Apt 17 - SF	02 Second Floor	45.6 m²	1B2P
Apt 18 - SF	02 Second Floor	57.3 m²	2B3P
Apt 19 - SF	02 Second Floor	45.3 m²	1B2P
Apt 20 - SF	02 Second Floor	45.8 m²	1B2P
20		959.2 m²	

Area Schedule (GIA)		Apartment Schedule	
Level	Area	Type	Count
00 Ground Floor	436.4 m²	1B2P	17
01 First Floor	432.6 m²	2B3P	3
02 Second Floor	428.9 m²	20	
	1297.9 m²		

P03	30/05/22	Minor amendment to entrance canopy and window fenestration to south elevation. Alterations to canopy following fire engineer feedback	DAL	DS
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P02	25/11/21	Planning Submission	DS	BR
P01	25/10/21	Issued For Client Feedback & Costing	DS	BR

Rev	Date	Description	Drawn by	Checked By
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Status	Purpose of Issue
S2	Planning

drawing stage **Developed Design**

client

**Great Places Housing Group**

project

**ELPM Island Site - Apartment Building**

drawing title

**Proposed Second Floor & Roof Plan - Apartments**

date 25/11/21 drawn DS  
scale @ A1 1 : 100 checked BR





Proposed West Elevation  
Scale @ 1 : 100



Proposed East Elevation  
Scale @ 1 : 100



Proposed North Elevation  
Scale @ 1 : 100



Proposed South Elevation  
Scale @ 1 : 100

NOTES

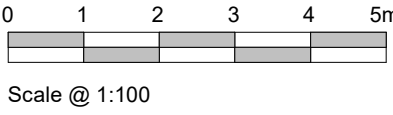
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  - All proprietary materials and products are to be used strictly in accordance with the manufacturers recommendations.
- This drawing contains the following model files: -  
12XXX-AEW-XX-XX-M3-A-0001

CDM 2015

Client notified of duties: **December 2020**  
Principal Designer: **CPC**

Unless noted below, all known hazards have been highlighted on the drawing:

- RWP Rain water downpipe  
RWO Rain water outlet  
SVP Soil Vent pipe  
AAV Air Admittance Valve  
WFB Wall mounted bathroom extract fan (min extract rate of 15 ltr/sec)  
CFB Cavity Fire Barrier  
WFK Wall mounted Kitchen extract fan (min extract rate of 60 ltr/sec)  
BF Boiler flue  
EXL Low Energy External light fitting with PIR sensor (min efficacy of 40 lumens per circuit watt) to comply with SED.  
L Glazing with laminated glass outer leaf  
T Glazing with toughened inner leaf  
E Egress windows to comply with building Regulations requirements  
OB Glazing with Obscure glass  
G Gas Meter  
E Electricity Meter  
RL Roof Light  
AH Access Hatch



P03	30/05/22	Minor amendment to entrance canopy and window fenestration to south elevation. Alterations to canopy following fire engineer feedback	DAL	DS
P02	25/11/21	Planning Submission	DS	BR
P01	25/10/21	Issued For Client Feedback & Costing	DS	BR
Rev	Date	Description	Drawn By	Checked By

Status: S2  
Purpose of Issue: For Information

drawing stage: Developed Design

client: Great Places Housing Group

project: ELPM Island Site - Apartment Building

drawing title: Proposed Apartment Elevations

date: 25.11.21  
scale@ A1: As indicated  
drawn: DS  
checked: BR

Rev: P03

12216-AEW-XX-XX-DR-A-0525

aew architects  
0161 214 4370  
www.aewarchitects.com



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**BURY COUNCIL**  
**DEPARTMENT FOR BUSINESS, GROWTH AND INFRASTRUCTURE**  
**PLANNING SERVICES**

**PLANNING CONTROL COMMITTEE**

**26 July 2022**

**SUPPLEMENTARY INFORMATION**

**Item:1 Land to east of M66 motorway, connecting Rochdale Road, Edenfield to Woodgate Hill WTW Compound, Bury Application No. 66947**  
**Proposed works for and use of replacement section of aqueduct, including earthworks and ancillary infrastructure including: new valve house buildings and kiosk with permanent vehicular access provision; installation of tunnel shafts and open cut connection areas within a temporary construction compound, to include site access, storage areas, plant and machinery, drainage infrastructure and a residents' parking area. In addition, mine grouting works, to facilitate main tunnelling works.**

**Highways issues** - The scheme provides a crossing point across the Castle Hill Road temporary construction access to ensure pedestrian access can be maintained along Castle Hill Road. A small car park would be provided if required off Castle Hill Road on the applicant's adjacent land, to be used by residents who have had parking displaced as a result of the proposed development. The scheme also provides a temporary relocated pedestrian refuge on Rochdale Old Road whilst the works are undertaken to allow construction vehicles to make the turn into Castle Hill Road. The temporary pedestrian refuge will be removed and then reinstated in its original location upon completion of the proposed works. These would all be delivered through a legal agreement. The applicant is offering and it is accepted that an Extraordinary agreement under s59 of the Highways Act 1980 would be undertaken and this is drafted. This would provide for a secured bonded fund to draw upon should works be required through the implementation of the scheme to public highways.

### **Conditions**

The full list of conditions is provided below:

1. The development must be begun not later than five years beginning with the date of this permission.

Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered BMBC-HW-APP-004-01-01 0, BMBC-HW-APP-004-01-02 1, BMBC-HW-APP-004-02 1, BMBC-HW-APP-004-04\_01 0, BMBC-HW-APP-004-04\_02 0, BMBC-HW-APP-004-04\_03 0, BMBC-HW-APP-004-04\_04 0, BMBC-HW-APP-004-04\_05 1, 80061155-01-JAC-TR5-97-DR-C-00007 P03, 80061155-01-JAC-TR5-97-DR-C-00008 P01, BMBC-HW-APP-004-05\_01 0, BMBC-HW-APP-004-05\_02 1, BMBC-HW-APP-004-06 1, BMBC-HW-APP-004-07\_01 0, BMBC-HW-APP-004-07\_02 0, 80061155-01-UU-TR5-XX-DR-C-20023 P02, BMBC-HW-APP-004-09 0, 80061155-01-UU-TR5-DR-C-00042 P02, 80061155-01-UU-TR5-97-DR-C-00043 P02, 8006115-01-UU-TR5-97-DR-C-00044 P02, 80061155-01-UU-TR5-97-DR-C-00047 P01, BMBC-HW-APP-004-11\_01 1, BMBC-HW-APP-004-13\_01 1, BMBC-HW-APP-004-13\_02 1, BMBC-HW-APP-004-13\_03 1, BMBC-HW-APP-004-13\_04 0, B27070CQ-JAC-XX-DR-C-TR6\_GA-2206 P01.4, B27070CQ-JAC-XX-DR-C-TR6\_VS-2007 P01.4, B27070CQ-JAC-XX-DR-C-TR6\_VT-2118 P01.1 and the Planning Design and Access Statement and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

3. Prior to the commencement of development, a phasing plan showing the sequencing of the development for managing the construction, restoration and reinstatement works shall be submitted to and agreed in writing by the Local Planning Authority. Conditions (set out below) that are required to be submitted to the Local Planning Authority for approval shall be submitted to and agreed in writing by the Local Planning Authority in accordance with the sequence set out in the agreed phasing plan. The development shall thereafter be implemented solely in accordance with the approved scheme or as subsequently amended by written agreement with the Local Planning Authority.

Reason. Details are required prior to the commencement of development in order to secure satisfactory detailed delivery of the highway works within each phase of development and across the site as a whole in accordance with Policy HT1 - A balanced Transportation Strategy of the Bury Unitary Development Plan.

4. Notwithstanding the plans and details submitted, development within each phase approved under Condition 3 shall not be commenced until details for all the temporary above ground structures associated with the respective compounds for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented during the course of the development.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

5. Notwithstanding the plans and details submitted, development within each phase approved under Condition 3 shall not be commenced until details for all the permanent above ground structures for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented during the course of the development.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

6. The respective compounds shall be removed from site to the written satisfaction of the Local Planning Authority within 6 months of it ceasing operation.

Reason. In the interests of the visual amenity pursuant to Policies H3/1 – Assessing Non-Conforming Uses and OL1/2 – New Buildings in the Green Belt of the Bury Unitary Development Plan.

7. Development within each phase approved under Condition 3 above shall not be commenced until:-

- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site for the relevant phase has been submitted to, and approved in writing by, the Local Planning Authority;
- Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider

environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

8. Following the provisions of Condition 7 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

9. Development within each phase approved under condition 3 shall not be commenced until a Materials Management Plan for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any works to include works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation work. The materials management plan shall be developed following the site investigations and risk assessments and shall:

- a. Identify all locations from which material will be excavated;
- b. Utilising the information contained within the contaminated land investigation, identify those areas of excavation which are contaminated
- c. For areas of excavation which may be subject to contamination estimate the volume of material arising, the approximate volumes of material to be remediated on site and provisional volume to be disposed of off-site;
- d. Illustrate where and how the remediation of contaminated material would take place;
- e. Illustrate where and how remediated material would be re-used, including volumetric calculations to demonstrate that the material can be accommodated within the proposed area of use and any measures for containment for this material;
- f. Detail the frequency of testing and testing specification for soils generated during the cut and fill operations, including how the materials are to be segregated and stored;
- g. Identify screening criteria for assessment of whether the materials can be reused without treatment or mitigation;
- h. For areas of excavation which are not subject to contamination provide the volume of material arising, and illustrate where and how non-contaminated material would be re-used including volumetric calculations to demonstrate that the material can be accommodated within the proposed area.

Once approved the materials management plan shall be implemented in its entirety.

Reason. To ensure the proposed development does not pose an unacceptable risk of pollution to controlled waters pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

10. Development within each phase approved under Condition 3 above shall not be commenced until a Site Waste Management Plan (SWMP) for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any

works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation. The Site Waste Management Plan shall include details of:

- a. the anticipated nature and volumes of waste that will be generated by construction work;
- b. the measures to minimise the generation of waste as a result of the proposed works;
- c. measures to maximise the re-use on-site of such waste;
- d. measures to be taken to ensure effective segregation at source of other waste arising during the carrying out of such works, including the provision of waste sorting, storage, recovery and recycling facilities as appropriate

The approved SWMP shall be implemented throughout the period of construction work on site.

Reason. To ensure the construction activities associated with the proposed development do not pose an unacceptable risk of pollution to controlled waters through the inappropriate management of waste on site pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

11. Development within each phase approved under Condition 3 above shall not be commenced until a scheme detailing how surface water flows and quality will be controlled and managed during the construction phase of the development for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. For the purposes of this condition the term 'construction work' shall be taken to include any works to include works to prepare the site for development including site access points, haul roads and compound areas but excluding site investigation. The construction phase surface water management plan shall include the following and be implemented before construction starts:

- a. An assessment of potential flows that would need to be managed during construction, including flows from the existing aqueduct, construction compounds and access roads and as a result of any groundwater dewatering or tunnelling activities;
- b. Details of the measures which would be put in place to capture, manage, treat and discharge flows from the component parts of the site identified in part a.
- c. A programme for the installation, maintenance and removal of the measures set out in part b. This should include provision for adapting the mitigation if it proves not to be effective.
- d. An assessment of potential contaminants which may be present in surface water runoff, and measures to segregate this surface or ground water from clean runoff;
- e. Assessment of potential options to retain, treat and remove potentially contaminated surface water runoff during the works;
- f. Details of a monitoring scheme to be implemented to confirm that no contaminants are present in runoff from the site intended for discharge to controlled waters (before, during and post construction)
- g. Details of how existing surface waters will be protected from any surface and ground waters generated

Once approved, the construction phase surface water management plan shall be implemented in its entirety and remain for the duration of the development. Should a need for amendments to the plan be required as a result of changing conditions, these must be submitted to and approved by the LPA.

Reason. To ensure the construction activities associated with the proposed

development do not pose an unacceptable risk of pollution to controlled waters and associated species and habitats pursuant to Policy EN7 - Pollution Control of the Bury Unitary Development Plan and the National Planning Policy Framework.

12. Development within each phase approved under Condition 3 above shall not be commenced until a scheme for the provision and management of any compensatory habitat necessary to mitigate the impacts of the project for the relevant phase has been submitted to, and agreed in writing by, the local planning authority and implemented as approved. Thereafter, the development shall be implemented in accordance with the approved scheme.

Reason. To ensure the protection of wildlife and supporting habitats and secure opportunities for enhancing the site's nature conservation value and delivering biodiversity net gain pursuant to Policy EN6 - Conservation of the Natural Environment and the National Planning Policy Framework

13. Development within each phase approved under Condition 3 above shall not be commenced until a scheme to ensure that;

- All private water supplies that may be impacted by the proposed development have been identified and any measures necessary to mitigate the impacts of the development on them for the relevant phase have been agreed with the LPA;

Thereafter, the development shall be implemented in accordance with the approved scheme.

Reason. To ensure that the proposed development does not harm the water environment in line with paragraph 174 of the National Planning Policy Framework

14. Development within each phase approved under condition 3 above shall not be commenced until a Traffic Management Plan (TMP) for the relevant phase has been submitted and approved in writing by the Local Planning Authority in association with the Local Highways Authority. The TMP shall include:

- latest works programme, to allow coordination and understanding of cumulative impact of other programmed works in the area (linked to Condition 3 above)
- The construction site traffic routes to be controlled by condition, the TMP should include a plan that clearly identifies each route
- Detailed design of Castle Hill Road Temporary Access
- Hours of operation and number of vehicle movements;
- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials;

Reason. Information not submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period and that the adopted highways are kept free of deposited material from the ground works operations, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

15. The routing of construction site traffic to/from the site will at all times, unless otherwise agreed in writing by the Local Planning Authority in consultation with the Local Highways Authority, be restricted to the following routes set out below:

**Woodgate Hill WTW Compound and Woodgate Hill mine grouting area**



M66 Junction 2 either from the north or south, Rochdale Road, Derby Way, B6222 Moorgate, B6222 Bell Lane, B6222 Rochdale Old Road, Castle Hill Road.  
[For up to approximately 12 weeks, via Second Avenue and then Sixth Avenue whilst the access on Castle Hill Road is constructed]

**White Carr Lane Mine Grouting Area**

From/to north Edenfield roundabout, A56 Wood Lane, A56 Whalley Road, A56 Manchester Road, A56 Walmersley Road, Walmersley Old Road, Bentley Lane, White Carr Lane.

From/to south M66, A56 Walmersley Road, Walmersley Old Road, Bentley Lane, White Carr Lane.

No other routes are to be used by site traffic unless otherwise agreed in writing by the Local Planning Authority in consultation with the Local Highways Authority.

Reason. To avoid pedestrian and highway conflict in relation to the construction of the development pursuant to Policy HT6/2 - Pedestrian/Vehicular Conflict of the Bury Unitary Development Plan.

16. Development within each phase approved under Condition 3 above shall not be commenced until a Construction Travel Plan for the relevant phase has been submitted to and approved in writing by the Local Planning Authority in association with the Local Highways Authority. The approved details shall be implemented before the respective phase commences.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent streets pursuant to Policy HT4 - New Development of the Bury Unitary Development Plan.

17. Development within each phase approved under Condition 3 above shall not be commenced until details of the location, design and specification of wheel-cleaning facilities or other measures to prevent the tracking out of material or debris onto the public highway for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. The wheel cleaning facilities or other measures approved pursuant to this condition shall be installed and thereafter maintained in working order and be used by all Heavy Goods Vehicles leaving the site throughout the construction and reinstatement phases.

Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy EN1/2 - Townscape and Built Development and Policy HT4 - New Development of the Bury Unitary Development Plan.

18. Development within each phase approved under Condition 3 above shall not be commenced until details of the location, design and specification of wheel-cleaning facilities or other measures to prevent the tracking out of material or debris onto the public highway for the relevant phase has been submitted to, and approved in writing by the Local Planning Authority. The wheel cleaning facilities or other measures approved pursuant to this condition shall be installed and thereafter maintained in working order and be used by all Heavy Goods Vehicles leaving the site throughout the construction and reinstatement phases.

Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy EN1/2 - Townscape and Built Development and Policy HT4 - New Development of the Bury Unitary Development Plan.

19. Development within each phase approved under Condition 3 above shall not be commenced until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works for the relevant phase. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by the local planning authority. The WSI shall cover the following:

1. Informed by the updated North West Regional Research Framework, a phased programme and methodology of investigation and recording to include:
  - i. archaeological evaluation trenching (based on the trenching plan supplied to Jacobs by GMAAS on 10th March 2021)
  - ii. informed by the above, more detailed targeted excavation (subject of a new WSI).
2. A programme for post investigation assessment to include:
  - i. analysis of the site investigations records and finds
  - ii. production of a final report on the investigation results
3. Deposition of the final report with the Greater Manchester Historic Environment Record
4. Dissemination of the results commensurate with their significance.
5. Provision for archive deposition of the report and records of the site investigation.
6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason. To record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible pursuant to Policies EN3/1 – Impact of Development on Archaeological Sites, EN3/2 – Development Affecting Archaeological Sites and EN3/3 – Ancient Monuments of the Bury Unitary Development Plan and Section 16 of the NPPF.

20. Development within each phase approved under Condition 3 above shall not be commenced until the mitigation measures for dust and air quality referred to in the Environmental Statement have been implemented for the relevant phase and are available for use.

Reason. To ensure the proposed development would not have an adverse impact upon air quality pursuant to Policy EN7/1 - Atmospheric Pollution of the Bury Unitary Development Plan.

21. No tree felling or vegetation clearance required to facilitate the scheme should be undertaken during the optimum time for bird nesting (March to August inclusive) unless nesting birds have been shown to be absent by a suitably qualified person.

Reason. All nesting birds, their eggs and young are legally protected under the terms of the Wildlife and Countryside Act 1981 (as amended) pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15 .

22. Any trees scheduled for removal as part of detailed plans for the scheme should be further inspected for the possible presence of bat roosts before any felling work commences. Surveys must be carried out by suitably qualified persons and to appropriate standards. If bats are found, measures must be proposed for the avoidance of harm to bats and for compensatory provision of replacement roosting sites.

Reason. All UK bats and their resting places are legally protected under the terms of the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species regulations 2019 (as amended) pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15

23. A precautionary survey of the works area for Badgers must be undertaken prior to work commencing. If the presence of badgers is recorded, a method statement must be prepared giving details of measures to be taken to avoid any possible harm to badgers

Reason. Badgers are mobile in their habits, and are protected under the terms of the Protection of Badgers Act 1992. There is suitable habitat in the works areas to support badgers pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15.

24. Development within each phase approved under condition 3 shall not be commenced until a detailed management / method statement for the relevant phase is submitted to and approved in writing by the Local Planning Authority giving details of measures to be taken to prevent the spread of the invasive plant Himalayan balsam during the course of the scheme.

The approved management / statement plan shall include a timetable for implementation and be submitted prior to ground works, affected by such invasive species, takes place. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.

Reason. The scheme does not provide full details of the actual extent of Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

25. Measures proposed for mitigating and compensating for the ecological harm which the scheme will cause as set out in the Environmental Statement (and in particular in the Construction Code of Practice (Appendix 3)) and Chapter 9 (Terrestrial and Aquatic Ecology)) are be required to be implemented as approved.

Reason. in the interests of protecting wildlife and the natural environment pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15.

26. Comprehensive Habitat and Landscape Management plans must be required to be prepared for new on-site landscapes and habitats. The Management plans and proposals should include details of the mechanism{s} by which the long-term implementation of the plan will be secured by the developer with the management body(ies) responsible for its delivery.

Reason. in the interests of landscape and habitat quality and sustainability pursuant to Bury UDP Policies EN6 and NPPF Chapter 15.

27. An Ecological Clerk of Works must be required to be appointed to oversee all aspects of the enabling, construction and reinstatement works.

Reason. In the interests of protecting wildlife and the natural environment pursuant to Bury UDP Policies EN6/3 and NPPF Chapter 15.

28. Development within each phase approved under Condition 3 above shall not be commenced until;

a) A scheme of targeted additional intrusive investigations (either in advance of the proposed works or during the mine stabilisation phase) has been agreed and carried out as required on site to establish the risks posed to the development by past coal mining activity.

b) any remediation works and/or mitigation measures to address land instability arising from coal mining legacy, as may be necessary, have been implemented on site in full in order to ensure that the site is safe and stable for the development proposed.

The intrusive site investigations and remedial works shall be carried out in accordance with authoritative UK guidance.

Reason. The scheme does not provide full details of the actual risk from coal mining, which is required to secure the satisfactory development of the site in relation to coal mining pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

29. Prior to the occupation of the development, or it being taken into beneficial use, a signed statement or declaration prepared by a suitably competent person confirming that the site is, or has been made, safe and stable for the approved development shall be submitted to the Local Planning Authority for approval in writing. This document shall confirm the methods and findings of the intrusive site investigations and the completion of any remedial works and/or mitigation necessary to address the risks posed by past coal mining activity.

Reason. The scheme does not provide full details of the actual risk from coal mining, which is required to secure the satisfactory development of the site in relation to coal mining pursuant to National Planning Policy Framework Section 15 - Conserving and enhancing the natural environment.

30. Development within each phase approved under Condition 3 above shall not be commenced until full details of the following have been submitted on a topographical survey of the site/adopted highways to, and agreed with, the Local Planning Authority:

- Scheme of highway works to a scope and specification to be agreed to relocate the affected pedestrian refuge at the Rochdale Old Road/Castle Hill Road junction following the undertaking of an appropriate swept path analysis exercise of manoeuvring heavy goods vehicles that will access the site via this route;
- Scheme of highway works to a scope and specification to be agreed to reinstate the affected pedestrian refuge in its original position upon completion of the development;
- Scheme of highway works to a scope and specification to be agreed to form the proposed site access onto Castle Hill Road, replacement footway and site access pedestrian crossing point following the undertaking of an appropriate swept path analysis exercise of manoeuvring heavy goods vehicles that will access the site via this junction;
- Scheme of highway works to a scope and specification to be agreed to reinstate the affected adopted highway at, and footway on, Castle Hill Road and abutting boundary wall upon completion of the development;
- Undertaking of an appropriate swept path analysis exercise of manoeuvring heavy goods vehicles that will access the site via the Bentley Lane/Public Footpath No. 15, Bury junction, and, if deemed necessary as a result of the exercise, appropriate scheme of highway works and pedestrian protection measures to a scope and specification to be agreed;
- Review of existing/need for the provision of new waiting/loading restrictions on the Second Avenue/Sixth Avenue, Roachdale Old Road/Castle Hill Road and Walmersley Old Road/Bentley Lane access routes to the site access points, incorporating measures to monitor and amended/supplement any implemented Traffic Regulation Orders.

The details subsequently approved shall be implemented to an agreed programme.

Reason. To ensure good highway and secure a safe access to the site in terms of highway safety and the flow of traffic in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

**Item:2      East Lancs Paper site, land between Cockclod Street/Cunliffe Street/Howard Street/Pollitt Street, Radcliffe Manchester, M26 9PG**  
**Application No. 67965**

Residential development of 27 no. houses and apartments with car parking, landscaped areas and associated works

The Development Manager recommended a site visit prior to the Planning Control Committee meeting.

**Conditions**

Condition 5 amended to read:

The construction of dwellings shall not commence unless and until a scheme for EV chargepoints (minimum 7kW\*) has been submitted to and approved by the Local Planning Authority. The approved scheme only shall thereafter be implemented and the EV chargepints maintained.

\*Mode 3, 7kW (32A) single phase, or 22kW (32A) three phase, and for 50kW Mode 4 rapid charging may be required. British Standard BS EN 61851-1:2011 to be used (note this version is due to be replaced by BS EN 61851-1:2019 on 5 July 2022).

Reason. This is to secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to the National Planning Policy Framework.

Condition 9 amended to read:

The construction of the dwellings shall not commence unless and until details/samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved by the Local Planning Authority. Only the approved materials shall be used for the construction of the development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Townscape and Built Design and H2/1 - The Form of New Residential Development.

Condition 15 amended to read:

Further to the submitted landscaping scheme, before construction of the dwellings, details shall also be provided to ensure that -

- the final planting schedules are in line with the habitats proposed in the net gain assessment;
- that a landscape and environmental management plan is provided to demonstrate how the conditions forecast will be achieved and maintained and;
- bat and bird box proposals provided.

The approved details and the approved landscaping scheme shall thereafter be implemented not later than 12 months from the date the building(s) is first occupied or within the first available tree planting season,; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted.

Reason. To secure the satisfactory development of the site and in the interests of



visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design and EN8/2 - Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 15 - Conserving and enhancing the natural environment of the NPPF.

The Affordable Housing Statement has been updated July 2022.  
Condition 16 therefore amended to read:

The development hereby approved shall only be developed by or on behalf of the applicant as units to be sold as shared ownership units or as general needs rent affordable housing. The affordable housing provision shall be provided in accordance with the approved Affordable Housing Statement, dated July 2022.

Reason. The proposed development has been granted given the particular circumstances of the applicant following a funding package which provides an opportunity to promote increased affordable housing and to ensure that the development would contribute to satisfying the need for affordable housing provision pursuant to Bury Unitary Development Plan Policy H4/1 - Affordable Housing and the associated Development Control Policy Guidance Note 5 - Affordable Housing Provision In New Residential Developments.

Condition 17 amended to read:

Notwithstanding the details indicated on the approved plans, no development other than remediation, demolition and enabling works shall commence unless and until full details of the following have been submitted on a topographical survey of the site and adjacent adopted highways to, and agreed with, the Local Planning Authority:

- Reconstruction of all affected footways/provision of 2.0m (minimum) new footways abutting the site on Cross Lane, Church Street East, Rectory Lane, Pollitt Street and Howard Street to a scope and specification to be agreed, incorporating the removal of the affected boundary fencing and landscaping features at the junction of Cock Clod Street with Cross Lane, demarcation of the limits of the existing/future adopted highway with flush (at all pedestrian/vehicular access points)/50mm upstand edgings (adjacent to all landscaped amenity areas) and all associated highway and highway drainage remedial works required as a result of the development;
- Provision of adoptable parallel parking lay-by arrangements on Pollitt Street to dimensions, and with entry/exit kerblines, to be agreed and all associated highway and highway drainage remedial works required as a result of the development;
- Revised arrangements at the junction of the un-named back street with Rectory Lane to form a 90 degree junction adjacent to the rear of Plot 7, and all associated highway and highway drainage remedial works required as a result of the development;
- Revised arrangements at the junction of the apartment car park access onto Howard Street with tracked kerb radii, measures to prevent vehicle encroachment from the adjacent disabled parking spaces, measures to prevent the discharge of surface water onto the adopted highway, demarcation of the limits of the adopted highway and all associated highway and highway drainage remedial works required as a result of the development;
- Provision of level pedestrian accesses at the interface with the existing adopted highway, including at either end of the unadopted pedestrian link between Cross Lane and Pollitt Street;
- If deemed redundant by United Utilities as part of the potential Public Open Space proposals at Rectory Lane/Church Street East, reinstatement of the existing access onto the adopted highway;

- Provision of a street lighting assessment of all adopted and unadopted highway used to serve the site and, if required, subsequent scheme of improvements to existing street lighting on the adopted/unadopted highways, incorporating the replacement of all affected street lighting columns as a result of the assessment and proposed access arrangements, in positions to be agreed.

The details subsequently approved shall be implemented to an agreed programme with all highway works completed prior to the development hereby approved being occupied/brought into use.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/1 - The Form of New residential development, H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict. and HT4 - New Development

Condition 21 amended to read:

The construction of the dwellings shall not commence unless and until details of the proposed arrangements for future management and maintenance of the proposed estate road and street lighting within the development have been submitted to and approved by the local planning authority. The estate road shall thereafter be maintained in accordance with the approved management and maintenance details until such time as a private management and maintenance company has been established.

Reason. To ensure that the unadopted estate road serving the development is maintained to an acceptable standard in the interests of residential/highway safety; to ensure a satisfactory appearance to the highways infrastructure serving the development; and to safeguard the visual amenities of the locality and users of the highway pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, H2/4 - Car Parking and New Development, HT6/2 - Pedestrian/Vehicular Conflict. and HT4 - New Development

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# REPORT FOR NOTING

Agenda Item 5

**Bury**  
COUNCIL

**Agenda  
Item**

**5**

<b>DECISION OF:</b>	<b>PLANNING CONTROL COMMITTEE</b>
<b>DATE:</b>	<b>26 July 2022</b>
<b>SUBJECT:</b>	<b>DELEGATED DECISIONS</b>
<b>REPORT FROM:</b>	<b>HEAD OF DEVELOPMENT MANAGEMENT</b>
<b>CONTACT OFFICER:</b>	<b>DAVID MARNO</b>
<b>TYPE OF DECISION:</b>	<b>COUNCIL</b>
<b>FREEDOM OF INFORMATION/STATUS:</b>	This paper is within the public domain
<b>SUMMARY:</b>	The report lists: Recent delegated planning decisions since the last PCC
<b>OPTIONS &amp; RECOMMENDED OPTION</b>	The Committee is recommended to the note the report and appendices
<b>IMPLICATIONS:</b>	
<b>Corporate Aims/Policy Framework:</b>	Do the proposals accord with the Policy Framework? Yes
<b>Statement by the S151 Officer: Financial Implications and Risk Considerations:</b>	Executive Director of Resources to advise regarding risk management
<b>Statement by Executive Director of Resources:</b>	N/A
<b>Equality/Diversity implications:</b>	No
<b>Considered by Monitoring Officer:</b>	N/A
<b>Wards Affected:</b>	All listed
<b>Scrutiny Interest:</b>	N/A

**TRACKING/PROCESS****DIRECTOR:**

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

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**1.0 BACKGROUND**

This is a monthly report to the Planning Control Committee of the delegated planning decisions made by the officers of the Council.

**2.0 CONCLUSION**

That the item be noted.

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**List of Background Papers:-None****Contact Details:-**

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Planning Services, Department for Resources and Regulation  
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Bury BL9 0EJ

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**Email: [d.marno@bury.gov.uk](mailto:d.marno@bury.gov.uk)**

# **Planning applications decided using Delegated Powers** **Between 20/06/2022 and 17/07/2022**



## Ward: **Bury East**

**Application No.:** 68153    **App. Type:** FUL    15/07/2022    Approve with Conditions  
**Location:** 86 Castle Hill Road, Bury, BL9 7RW  
**Proposal:** Change of use of land at rear to form amenity area and rear garden; single storey rear extension; front porch

**Application No.:** 68299    **App. Type:** FUL    05/07/2022    Approve with Conditions  
**Location:** 78 Heywood Street, Bury, BL9 7EA  
**Proposal:** Single storey rear extension

**Application No.:** 68360    **App. Type:** FUL    11/07/2022    Approve with Conditions  
**Location:** 44 Shepherd Street, Bury, BL9 0RT  
**Proposal:** Dormer extension at rear

**Application No.:** 68363    **App. Type:** FUL    01/07/2022    Approve with Conditions  
**Location:** 12 Bamford Close, Bury, BL9 7TR  
**Proposal:** Front dormer

**Application No.:** 68370    **App. Type:** FUL    11/07/2022    Approve with Conditions  
**Location:** 29 Chestnut Avenue, Bury, BL9 7QW  
**Proposal:** Single storey side/rear extension

## Ward: **Bury East - Moorside**

**Application No.:** 68317    **App. Type:** LDCP    28/06/2022    Lawful Development  
**Location:** Suite 12a, 79-81 Imperial House, Hornby Street, Bury, BL9 5BN  
**Proposal:** Certificate of lawfulness for a proposed use permitting the occupation of the premises which falls within use Class E(g)(i) (office) for the purpose of undertaking the administrative functions of an app based private hire vehicle operator

**Application No.:** 68401    **App. Type:** FUL    27/06/2022    Approve with Conditions  
**Location:** 305 Walmersley Road, Bury, BL9 6NX  
**Proposal:** Single storey extension at rear

## Ward: **Bury East - Redvales**

**Application No.:** 67794    **App. Type:** FUL    04/07/2022    Approve with Conditions  
**Location:** 24B Meadway, Bury, BL9 9TY  
**Proposal:** Roof extension to facilitate rear dormer extension; Widening of dropped kerb



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**Application No.:** 68064      **App. Type:** LDCP      24/06/2022      Lawful Development  
**Location:** Land south of the industrial area used by DHL, between the River Irwell, Bealey's Goit and Swan Lodge  
**Proposal:** Lawful Development Certificate for a proposed approximately 200m linear length drainage channel/swale with embankments; access track with post and wire fence.

Ward: **Bury West - Elton**

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**Application No.:** 68252      **App. Type:** FUL      05/07/2022      Approve with Conditions  
**Location:** 10 Tottington Road, Bury, BL8 1LH  
**Proposal:** Change of use and alterations to principal elevation of former shop (Use Class E) to form a four bedroom dwellinghouse (Use Class C3)

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**Application No.:** 68364      **App. Type:** FUL      06/07/2022      Approve with Conditions  
**Location:** 9 Valley Avenue, Bury, BL8 1FA  
**Proposal:** Single storey front extension

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**Application No.:** 68376      **App. Type:** FUL      23/06/2022      Approve with Conditions  
**Location:** 6 Balcombe Close, Bury, BL8 4PL  
**Proposal:** Two storey extension at side/rear; Front porch

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**Application No.:** 68458      **App. Type:** LDCP      14/07/2022      Lawful Development  
**Location:** 66 Bankhouse Road, Bury, BL8 1DS  
**Proposal:** Lawful development certificate for proposed single storey extension at rear

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Ward: **Bury West - West**

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**Application No.:** 68186      **App. Type:** FUL      21/06/2022      Approve with Conditions  
**Location:** 22 Turton Close, Bury, BL8 2EE  
**Proposal:** Part single/Part first floor extension at rear; Raised decking at rear.

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**Application No.:** 68300      **App. Type:** FUL      24/06/2022      Approve with Conditions  
**Location:** 108 & 110 Ainsworth Road, Bury, BL8 2RS  
**Proposal:** Change of use of No.108 Dwelling (class C3) to dental clinic (class D) merge with existing dental clinic at No.110, Window to front elevation; 4 new parking bays at rear

---

**Application No.:** 68342      **App. Type:** FUL      13/07/2022      Approve with Conditions  
**Location:** 606 Bolton Road, Bury, BL8 2DR  
**Proposal:** Hip to gable roof extension and loft conversion with windows to side gable wall including feature window to top of gable

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**Application No.:** 68372      **App. Type:** FUL      08/07/2022      Approve with Conditions  
**Location:** 8 Harwood Drive, Bury, BL8 2ED  
**Proposal:** Single storey side/rear extension

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**Application No.:** 68383      **App. Type:** FUL      11/07/2022      Approve with Conditions  
**Location:** 353 Ainsworth Road, Bury, BL8 2LU  
**Proposal:** Part single/Part two storey extension at side; Single storey extension at rear

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**Application No.:** 68390      **App. Type:** FUL      13/07/2022      Approve with Conditions  
**Location:** 37 Ashington Drive, Bury, BL8 2TS  
**Proposal:** Single storey extension at rear; New porch at side.

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**Application No.:** 68406      **App. Type:** LDCP      28/06/2022      Lawful Development  
**Location:** 9 Sutton Close, Bury, BL8 2EL  
**Proposal:** Lawful development certificate for proposed single storey rear extension

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Ward: **North Manor**

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**Application No.:** 67882      **App. Type:** FUL      08/07/2022      Approve with Conditions  
**Location:** 4 Tor Avenue, Tottington, Bury, BL8 4HG  
**Proposal:** Demolition of existing dwelling and construction of replacement dwelling with associated hard and soft landscaping

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**Application No.:** 68274      **App. Type:** FUL      21/06/2022      Approve with Conditions  
**Location:** 24 Brookside Crescent, Tottington, Bury, BL8 4BG  
**Proposal:** Part single/part two storey extension at side; Canopy at front

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**Application No.:** 68279      **App. Type:** FUL      30/06/2022      Approve with Conditions  
**Location:** 15 Brooklands Road, Ramsbottom, Bury, BL0 9SW  
**Proposal:** Increase height to create a two storey dwelling plus additional two storey build at rear; External alterations and rendered elevations, two parking spaces at front.

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**Application No.:** 68323      **App. Type:** FUL      11/07/2022      Approve with Conditions  
**Location:** 6 Stanmere Court, Tottington, Bury, BL8 4JQ  
**Proposal:** Single storey extension at rear

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**Application No.:** 68356      **App. Type:** FUL      11/07/2022      Approve with Conditions  
**Location:** 11 Brookfield Road, Bury, BL9 5LA  
**Proposal:** Single storey extensions at front and rear, new windows on side elevation, covered carport area, raised patio at rear.

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**Application No.:** 68393      **App. Type:** FUL      13/07/2022      Approve with Conditions  
**Location:** 1 Mayfield Close, Ramsbottom, Bury, BL0 9TL  
**Proposal:** Single storey extension at side

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**Application No.:** 68404      **App. Type:** FUL      23/06/2022      Approve with Conditions  
**Location:** 4 Mansfield Avenue, Ramsbottom, Bury, BL0 9US  
**Proposal:** Part single/two storey side extension; Two storey/single storey rear extension; Front bay window; Front porch

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**Application No.:** 68457    **App. Type:** FUL    01/07/2022    Approve with Conditions  
**Location:** Farm Shop & Cafe, Falshaws Farm, Rowlands Road, Bury, BL9 5LJ  
**Proposal:** Single storey extension at rear to be used for seating and shop display

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**Application No.:** 68483    **App. Type:** AG    08/07/2022    Prior Approval Required and Refused  
**Location:** Land off Woodhey Road, Ramsbottom, Bury, BL0 9RD  
**Proposal:** Prior approval for new agricultural building for the storage of hay and equipment.

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Ward: **Prestwich - Holyrood**

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**Application No.:** 68249    **App. Type:** FUL    14/07/2022    Approve with Conditions  
**Location:** 55 Milton Road, Prestwich, Manchester, M25 1PT  
**Proposal:** Replacement of garage with new single storey garage building

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**Application No.:** 68327    **App. Type:** FUL    21/06/2022    Split Decision  
**Location:** 15 Orange Hill Road, Prestwich, Manchester, M25 1LR  
**Proposal:** A: Front Porch and amendments to front bay window  
B: Rear dormer extensions

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**Application No.:** 68357    **App. Type:** FUL    05/07/2022    Approve with Conditions  
**Location:** 102 Cuckoo Lane, Prestwich, Manchester, M25 2NG  
**Proposal:** Single storey extension at rear and Dormer extension at rear

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**Application No.:** 68551    **App. Type:** GPDE    13/07/2022    Prior Approval Not Required - Extension  
**Location:** 20 St Josephs Avenue, Whitefield, Manchester, M45 6NT  
**Proposal:** Prior approval for proposed single storey rear extension

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Ward: **Prestwich - Sedgley**

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**Application No.:** 68050    **App. Type:** FUL    04/07/2022    Approve with Conditions  
**Location:** 52 Watkins Drive, Prestwich, Manchester, M25 0DS  
**Proposal:** Proposed rear two storey extension, loft extension and raising of ridge height; Dormer extension at rear

---

**Application No.:** 68057    **App. Type:** FUL    24/06/2022    Approve with Conditions  
**Location:** 52 Bury Old Road, Prestwich, Manchester, M25 0ER  
**Proposal:** Extension of temporary permission of 3 years for retention of existing portacabin for use as a classroom with the addition of main entrance extension and toilet facility at rear; Alterations to car park layout

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**Application No.:** 68233    **App. Type:** FUL    23/06/2022    Approve with Conditions  
**Location:** 5 Blackburn Street, Prestwich, Manchester, M25 1FT  
**Proposal:** Single storey extension at rear

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<b>Application No.:</b>	68237	<b>App. Type:</b>	FUL	01/07/2022	Approve with Conditions
<b>Location:</b>	4 Windsor Road, Prestwich, Manchester, M25 0DZ				
<b>Proposal:</b>	Single storey extension at side				
<b>Application No.:</b>	68243	<b>App. Type:</b>	FUL	01/07/2022	Approve with Conditions
<b>Location:</b>	44 and 46 Meade Hill Road, Prestwich, Manchester, M25 0DJ				
<b>Proposal:</b>	Two storey side extension at no. 46 and single storey side joint extension to link no. 44 and 46				
<b>Application No.:</b>	68262	<b>App. Type:</b>	FUL	24/06/2022	Approve with Conditions
<b>Location:</b>	3 Winchester Avenue, Prestwich, Manchester, M25 0LJ				
<b>Proposal:</b>	Single storey extension at side; Front porch; Amended access for parking				
<b>Application No.:</b>	68297	<b>App. Type:</b>	FUL	30/06/2022	Approve with Conditions
<b>Location:</b>	11 Lancaster Drive, Prestwich, Manchester, M25 0HZ				
<b>Proposal:</b>	Single storey rear extension				
<b>Application No.:</b>	68332	<b>App. Type:</b>	FUL	21/06/2022	Approve with Conditions
<b>Location:</b>	17 Castle Hill Road, Prestwich, Manchester, M25 0FR				
<b>Proposal:</b>	Single storey extensions at side and rear				
<b>Application No.:</b>	68344	<b>App. Type:</b>	FUL	11/07/2022	Approve with Conditions
<b>Location:</b>	82 Meade Hill Road, Prestwich, Manchester, M8 4LP				
<b>Proposal:</b>	Single storey rear extension including link to garage				
<b>Application No.:</b>	68405	<b>App. Type:</b>	FUL	27/06/2022	Approve with Conditions
<b>Location:</b>	3 Belfield Road, Prestwich, Manchester, M25 0FN				
<b>Proposal:</b>	First floor extension at rear.				
<b>Application No.:</b>	68479	<b>App. Type:</b>	FUL	13/07/2022	Approve with Conditions
<b>Location:</b>	52 Brooklands Road, Prestwich, Manchester, M25 0ED				
<b>Proposal:</b>	Single storey extension at side and First floor extension at rear				
<b>Application No.:</b>	68493	<b>App. Type:</b>	FUL	13/07/2022	Approve with Conditions
<b>Location:</b>	43 Hereford Drive, Prestwich, Manchester, M25 0AG				
<b>Proposal:</b>	Hip to gable roof extension with loft conversion and rear dormer; Single storey side extension, first floor rear extension and front porch				
<b>Application No.:</b>	68559	<b>App. Type:</b>	GPDE	13/07/2022	Prior Approval Not Required - Extension
<b>Location:</b>	30 Hereford Drive, Prestwich, Manchester, M25 0AG				
<b>Proposal:</b>	Prior approval for proposed single storey rear extension				



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**Application No.:** 68566      **App. Type:** GPDE      13/07/2022      Prior Approval Not Required - Extension  
**Location:** 29 Eastleigh Road, Prestwich, Manchester, M25 0BX  
**Proposal:** Prior approval for proposed single storey rear extension

Ward: **Prestwich - St Mary's**

---

**Application No.:** 68143      **App. Type:** FUL      28/06/2022      Approve with Conditions  
**Location:** 20 Prestwich Park Road South, Prestwich, Manchester, M25 9PE  
**Proposal:** Proposed basement access and Raised decking at rear

---

**Application No.:** 68231      **App. Type:** FUL      24/06/2022      Refused  
**Location:** Glenside, St Anns Road, Prestwich, Manchester, M25 9QE  
**Proposal:** Two storey side extension with gable roof and raised decking at rear.

---

**Application No.:** 68268      **App. Type:** FUL      24/06/2022      Approve with Conditions  
**Location:** International House, Church Lane, Prestwich, M25 1AN  
**Proposal:** Change of use from offices to 8no. apartments.

---

**Application No.:** 68310      **App. Type:** FUL      21/06/2022      Approve with Conditions  
**Location:** 11 Woodward Road, Prestwich, Manchester, M25 9TX  
**Proposal:** Single storey / two storey extension at side and single storey extension at rear

---

**Application No.:** 68392      **App. Type:** FUL      06/07/2022      Approve with Conditions  
**Location:** 1 Kingswood Road, Prestwich, Manchester, M25 3AB  
**Proposal:** Single storey extension at rear

---

**Application No.:** 68487      **App. Type:** FUL      01/07/2022      Approve with Conditions  
**Location:** 8 Barnhill Road, Prestwich, Manchester, M25 9NH  
**Proposal:** Two storey extension at front; Roof alterations including pitch roof to existing two storey flat roof side/rear extension and Garage conversion

Ward: **Radcliffe - North and Ainsworth**

---

**Application No.:** 68324      **App. Type:** PMBPA      28/06/2022      Prior Approval Required and Granted  
**Location:** 344 Bradley Fold Road, Radcliffe, Bolton, BL2 6RL  
**Proposal:** Prior approval for the change of use and associated operational development required to change an agricultural building into 1no. dwelling

---

**Application No.:** 68338      **App. Type:** FUL      23/06/2022      Approve with Conditions  
**Location:** 28 Sumner Avenue, Ainsworth, Radcliffe, Bolton, BL2 5RQ  
**Proposal:** Conversion of detached outbuilding to living accommodation

---

**Application No.:** 68340      **App. Type:** FUL      23/06/2022      Approve with Conditions  
**Location:** 8 Sherbourne Close, Radcliffe, Manchester, M26 3UP  
**Proposal:** Single storey rear extension

---

**Application No.:** 68358      **App. Type:** FUL      04/07/2022      Approve with Conditions  
**Location:** 16 Tipton Close, Radcliffe, Manchester, M26 3WZ  
**Proposal:** Single storey extension at side/rear

Ward: **Radcliffe - West**

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**Application No.:** 68266      **App. Type:** FUL      04/07/2022      Approve with Conditions  
**Location:** 14 Ringley Road West, Radcliffe, Manchester, M26 1DJ  
**Proposal:** Two storey extension at side; Single storey extension at rear.

Ward: **Ramsbottom + Tottington - Tottington**

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**Application No.:** 68103      **App. Type:** FUL      24/06/2022      Refused  
**Location:** 29 Chestnut Avenue, Tottington, Bury, BL8 3EE  
**Proposal:** Two storey extension at rear; First floor extension at side

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**Application No.:** 68276      **App. Type:** FUL      04/07/2022      Approve with Conditions  
**Location:** 17 Claughton Road, Tottington, Bury, BL8 3BS  
**Proposal:** Single storey extension at side/rear

---

**Application No.:** 68284      **App. Type:** FUL      08/07/2022      Approve with Conditions  
**Location:** Kirklees Lodge, Brandlesholme Road, Bury, BL8 4LS  
**Proposal:** A ground mounted solar panel system

Ward: **Ramsbottom and Tottington - Ramsbottom**

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**Application No.:** 68181      **App. Type:** FUL      05/07/2022      Approve with Conditions  
**Location:** 131-133 Whalley Road, Ramsbottom, Bury, BL0 0DG  
**Proposal:** Change of use of ground floor shop to residential (Class C3)

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**Application No.:** 68222      **App. Type:** FUL      11/07/2022      Approve with Conditions  
**Location:** 4 Shipper Bottom Lane, Ramsbottom, Bury, BL0 0DB  
**Proposal:** Two storey extension at side, single storey extension at rear, steps to front entrance and raised patio area at rear, fence 2.1m high alongside patio

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**Application No.:** 68288      **App. Type:** FUL      24/06/2022      Approve with Conditions  
**Location:** Grant Arms Hotel, 11 Market Place, Ramsbottom, Bury, BL0 9AJ  
**Proposal:** Proposed change of use from existing public house (Sui Generis) to veterinary practice (Class E)

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**Application No.:** 68289      **App. Type:** LBC      24/06/2022      Approve with Conditions  
**Location:** Grant Arms Hotel, 11 Market Place, Ramsbottom, Bury, BL0 9AJ  
**Proposal:** Listed Building Consent for proposed change of use

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**Application No.:** 68336      **App. Type:** FUL      21/06/2022      Approve with Conditions  
**Location:** Bast House, Manchester Road, Ramsbottom, Bury, BL9 5LZ  
**Proposal:** Variation of condition 02 following approval of planning permission 66016-Garage widened to provide internal corridor and WC and the back door position amended accordingly; position of window GW-03 moved (Ground Floor WC and additional ensuite to First Floor Bedroom 3); addition of window GW-05 to boot room / cloaks entrance and introduction of a stove to the kitchen - additional flue indicated on the elevations and internal amendments to layout

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**Application No.:** 68337      **App. Type:** FUL      27/06/2022      Approve with Conditions  
**Location:** Bast House Barns, Manchester Road, Ramsbottom, Bury, BL9 5LZ  
**Proposal:** Variation of condition 02 following approval of planning permission 60870:- Amendments to internal layout of unit 2; First floor - additional bedroom and ensuite; Second floor - additional floor area to provide dressing room & ensuite and East elevation amended to reflect the above changes.

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**Application No.:** 68384      **App. Type:** FUL      08/07/2022      Approve with Conditions  
**Location:** 5 York Drive, Ramsbottom, Bury, BL0 9QD  
**Proposal:** Single storey extensions at side and rear; Loft conversion with dormer extension at rear

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**Application No.:** 68410      **App. Type:** FUL      06/07/2022      Approve with Conditions  
**Location:** 23 Falstone Avenue, Ramsbottom, Bury, BL0 9LX  
**Proposal:** Two storey rear extension; 2 no first floor side frosted windows to be added

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**Ward: Whitefield + Unsworth - Besses**

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**Application No.:** 67754      **App. Type:** FUL      14/07/2022      Approve with Conditions  
**Location:** 8 Mayfair Grove, Whitefield, Manchester, M45 6BQ  
**Proposal:** Fencing to the side

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**Application No.:** 68080      **App. Type:** GPDE      11/07/2022      Prior Approval Not Required - Extension  
**Location:** 15 Willow Close, Bury, BL9 8NU  
**Proposal:** Prior approval for proposed single storey rear extension

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**Application No.:** 68194      **App. Type:** FUL      14/07/2022      Approve with Conditions  
**Location:** 9 Brook Drive, Whitefield, Manchester, M45 8FR  
**Proposal:** Single storey extension at rear

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**Application No.:** 68303      **App. Type:** FUL      08/07/2022      Approve with Conditions  
**Location:** 14 Victoria Square, Whitefield, Manchester, M45 6AL  
**Proposal:** Change of use from Shop (Class E) to hot food takeaway (Sui Generis); Alterations to shop front; Extraction flue to rear.

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**Application No.:** 68365    **App. Type:** FUL    05/07/2022    Approve with Conditions  
**Location:** 53 Cunningham Drive, Bury, BL9 8PD  
**Proposal:** Single storey side/rear extension

Ward: **Whitefield + Unsworth - Pilkington Park**

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**Application No.:** 68073    **App. Type:** FUL    30/06/2022    Approve with Conditions  
**Location:** 297 Bury New Road, Whitefield, Manchester, M45 7SE  
**Proposal:** Two storey/single storey rear extension: Additional window to first floor front elevation

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**Application No.:** 68326    **App. Type:** FUL    23/06/2022    Approve with Conditions  
**Location:** 6 Birch Avenue, Whitefield, Manchester, M45 7HW  
**Proposal:** Two storey side extension

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**Application No.:** 68373    **App. Type:** FUL    23/06/2022    Approve with Conditions  
**Location:** 12 Hillingdon Road, Whitefield, Manchester, M45 7QN  
**Proposal:** New porch at front; First floor extension at side/rear and Alterations to front elevation/roof

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**Application No.:** 68387    **App. Type:** FUL    11/07/2022    Approve with Conditions  
**Location:** 159 Radcliffe New Road, Radcliffe, Manchester, M45 7RG  
**Proposal:** Single storey extensions at rear

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**Application No.:** 68422    **App. Type:** FUL    11/07/2022    Approve with Conditions  
**Location:** 7 Ringley Close, Whitefield, Manchester, M45 7HR  
**Proposal:** Hip to gable loft conversion with front and rear dormers and velux windows; Raise roof ridge height

Ward: **Whitefield + Unsworth - Unsworth**

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**Application No.:** 68066    **App. Type:** FUL    14/07/2022    Approve with Conditions  
**Location:** Unsworth Primary School, Blackley Close, Bury, BL9 8LY  
**Proposal:** Proposed additional staff car parking (7 additional spaces)

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**Application No.:** 68263    **App. Type:** FUL    22/06/2022    Refused  
**Location:** 7 Burndale Drive, Bury, BL9 8EN  
**Proposal:** Two storey side extension with gable roof plus new parking layout

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**Application No.:** 68322    **App. Type:** FUL    28/06/2022    Approve with Conditions  
**Location:** 266 Parr Lane, Bury, BL9 8LU  
**Proposal:** Two storey side extension with velux windows; First floor side obscure glazed window; Raised decking to rear



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**Application No.:** 68391      **App. Type:** FUL      06/07/2022      Approve with Conditions  
**Location:** 47 Ajax Drive, Bury, BL9 8EF  
**Proposal:** Detached outbuilding and raised decking at rear

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**Total Number of Applications Decided:**      83

# REPORT FOR NOTING

Agenda Item 6

**Bury**  
COUNCIL

**Agenda  
Item**

**6**

<b>DECISION OF:</b>	<b>PLANNING CONTROL COMMITTEE</b>
<b>DATE:</b>	<b>26 July 2022</b>
<b>SUBJECT:</b>	<b>PLANNING APPEALS</b>
<b>REPORT FROM:</b>	<b>HEAD OF DEVELOPMENT MANAGEMENT</b>
<b>CONTACT OFFICER:</b>	<b>DAVID MARNO</b>
<b>TYPE OF DECISION:</b>	<b>COUNCIL</b>
<b>FREEDOM OF INFORMATION/STATUS:</b>	This paper is within the public domain
<b>SUMMARY:</b>	<p>Planning Appeals:</p> <ul style="list-style-type: none"> <li>- Lodged</li> <li>- Determined</li> </ul> <p>Enforcement Appeals</p> <ul style="list-style-type: none"> <li>- Lodged</li> <li>- Determined</li> </ul>
<b>OPTIONS &amp; RECOMMENDED OPTION</b>	The Committee is recommended to the note the report and appendices
<b>IMPLICATIONS:</b>	
<b>Corporate Aims/Policy Framework:</b>	Do the proposals accord with the Policy Framework? Yes
<b>Statement by the S151 Officer: Financial Implications and Risk Considerations:</b>	Executive Director of Resources to advise regarding risk management
<b>Statement by Executive Director of Resources:</b>	N/A
<b>Equality/Diversity implications:</b>	No
<b>Considered by Monitoring Officer:</b>	N/A

<b>Wards Affected:</b>	All listed
<b>Scrutiny Interest:</b>	N/A

## TRACKING/PROCESS

## DIRECTOR:

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

### 1.0 BACKGROUND

This is a monthly report to the Committee of the Planning Appeals lodged against decisions of the authority and against Enforcement Notices served and those that have been subsequently determined by the Planning Inspectorate.

Attached to the report are the Inspectors Decisions and a verbal report will be presented to the Committee on the implications of the decisions on the Appeals that were upheld.

### 2.0 CONCLUSION

That the item be noted.

### List of Background Papers:-

#### Contact Details:-

David Marno, Head of Development Management  
Planning Services, Department for Resources and Regulation,  
3 Knowsley Place ,Bury BL9 0EJ

**Tel: 0161 253 5291**

**Email: [d.marno@bury.gov.uk](mailto:d.marno@bury.gov.uk)**

**Planning Appeals Lodged  
between 20/06/2022 and 17/07/2022**



**Application No.:** 65844/FUL

**Appeal lodged:** 06/07/2022

**Decision level:** COM

**Appeal Type:** Written Representations

**Recommended Decision:** Minded to Approve

**Applicant:** Ecclestone Homes Limited

**Location** Former Mondi/Holcombe Mill, Bridge Street, Ramsbottom, Bury, BL0 0BS

**Proposal** Erection of 73 no. dwellings including the retention and conversion of 2 existing buildings to residential use (5 no. units), the retention of a chimney and the demolition of a derelict building, together with engineering operations to create a development platform and associated parking, landscaping, drainage, the layout of internal estate roads and footways and other associated works

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**Total Number of Appeals Lodged: 1**



**Planning Appeals Decided  
between 20/06/2022 and 17/07/2022**



**Application No.:** 67956/FUL

**Decision level:** DEL

**Recommended Decision:** Refuse

**Applicant:** Jacobs

**Location:** 69 Richmond Avenue, Prestwich, Manchester, M25 0LW

**Proposal:** Loft conversion with hip to gable roof extension; Front and rear dormers

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**Appeal Decision:** Dismissed

**Date:** 30/06/2022

**Appeal type:** Written Representations

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## Appeal Decision

Site visit made on 29 June 2022

**by D Hartley BA (Hons) MTP MBA MRTPI**

**an Inspector appointed by the Secretary of State**

**Decision date: 30<sup>TH</sup> June 2022**

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**Appeal Ref: APP/T4210/D/22/3299383**

**69 Richmond Avenue, Prestwich M25 0LW**

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
  - The appeal is made by Mr Yomtov Eliezer Jacobs against the decision of Bury Metropolitan Borough Council.
  - The application Ref 67956, dated 6 January 2022, was refused by notice dated 3 March 2022.
  - The development proposed is described as a loft conversion, hip to gable and front and rear roof dormers.
- 

### Decision

1. The appeal is dismissed.

### Procedural Matter

2. The Council's refusal notice describes the appeal development as a '*loft conversion with hip to gable roof extension; front and rear dormers*'. This more accurately describes the proposal. I have therefore determined the appeal on this basis.

### Main Issue

3. The main issue is the effect of the development on the character and appearance of the area.

### Reasons

4. There is a general design consistency to the dwellings in the immediate locality including the use of brick elevations and boundary front walls, imposing two storey projecting gable bays with small, pitched roofs and with public facing main roofs slopes that are predominantly unbroken. These attributes add positively and distinctively to the character and appearance of the area.
5. The appeal property is a semi-detached dwelling and the surrounding area is mainly residential in character. It is a corner plot and its design is slightly different to other dwellings in Richmond Avenue and Mowbray Avenue. Nevertheless, it includes much of the aforementioned distinctive attributes and, in this regard, adds positively and distinctively to the general design uniformity in the immediate area.
6. In conjunction with the provision of a new hip to gable roof, the proposed rear dormer would appear dominant and top heavy on the host property. It would occupy a significant proportion of the rear roof slope. Owing to its position,

scale and design, it would be appreciated by passers-by as an incongruous box structure at odds with the otherwise mainly unbroken roof slopes that can be seen at close distance from public roads. I acknowledge that there are dormers to the rear of other properties in the area including a two storey extension with rear dormers at 55 Richmond Avenue. However, in contrast to the rear of other properties in the street, the appeal property is much more prominent and conspicuous when seen from Mowbray Avenue. In other words, all parts of this property are sensitive to change in so far that they are visible from public vantage points.

7. The above harm would be compounded by the fact that the proposal includes an increase in the height of the projecting side gable. The impact would be that of creating an overly fussy, discordant and dominant extension to the detriment of the appearance of the host property and the prevailing character and appearance of the street-scene.
8. In addition to the above, the development includes a front roof dormer. This addition would add to the overall dominance of the development as whole when seen from Mowbray Avenue. While the front dormer would not be as large as the proposed rear dormer, it would nevertheless unacceptably compete with the two storey projecting pitched roof gables belonging to the pair of semi-detached dwellings, particularly when seen by those approaching the site from Albert Avenue and into Mowbray Avenue. Furthermore, it would have the effect of uncharacteristically breaking up the existing main roof space and introducing an out of keeping roof addition that would be noticeable when viewed from public areas. To this extent, the proposal would have a discordant impact on the character and appearance of the area.
9. For the collective reasons outlined above, I therefore conclude that when the proposal is considered as a whole, it would cause significant harm to the character and appearance of the area. To this extent, it would not accord with the design, character and appearance requirements of saved policy H2/3 of the Bury Unitary Development Plan 1997, the Council's Supplementary Planning Document 6: Alterations and Extensions to Residential Properties (updated 2010) and Chapter 12 of the National Planning Policy Framework 2021.

### **Other Matters**

10. The appellant considers that a precedent has been set in terms of similar development at 47 Kings Road. While this dwelling is not very far away from the appeal property, it is nevertheless seen within a street which has a different character and appearance. The appeal property is seen within Richmond Avenue and Mowbray Avenue which has a number of different and distinctive characteristics. In this context, I have found that the proposal would be harmful. In any event, I do not consider that the development at 47 Kings Road can reasonably be said to amount to good design given the size, bulk and appearance of the box dormers. Furthermore, the appellant's reference to development approved at 2 Lincoln Drive and 14 Kings Road is not seen without the same context as the appeal site.
11. The appellant has drawn my attention to box dormers to the rear of 42 Richmond Road which is also a corner plot. Box dormers are nevertheless not a prevalent feature when seen from public areas in Richmond Road. I acknowledge that it would appear that this development was approved by the

Council, but nevertheless it stands out as an incongruous form of development when seen in its immediate setting.

12. I have determined this appeal on its individual planning merits and the examples of other development referred to by the appellant does not lead me to find that the harmful proposal should nevertheless be allowed.
13. I note the supportive comments made by other interested parties and do not doubt the appellant's requirement for additional floorspace. I acknowledge that it would likely be possible to make changes to the property utilising permitted development rights. I do not know if such permitted development rights would be exercised if this appeal were to be dismissed. However, it would not be possible to construct the whole of the proposed scheme utilising permitted development rights and so I afford limited weight to any theoretical permitted development fall-back position. That said, the existence of permitted development rights for the property suggests to me that it might be possible to carry out some sensitive and alternative development alterations to the property, thereby addressing the appellant's desire for increased accommodation.
14. The aforementioned other matters need to be balanced against the significant harm that would be caused by the proposal to the character and appearance of the area. In this case, I find that a refusal of planning permission is a necessary requirement in the public interest. The other matters raised, including supportive comments made by an interested party, are not of significant weight or magnitude to alter or outweigh my conclusion on the main issue.

### **Conclusion**

15. For the reasons given above, I conclude that the development would not accord with the development plan for the area taken as a whole and there are no material considerations that indicate the decision should be made other than in accordance with the development plan. Therefore, the appeal should be dismissed.

*D Hartley*

INSPECTOR



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